

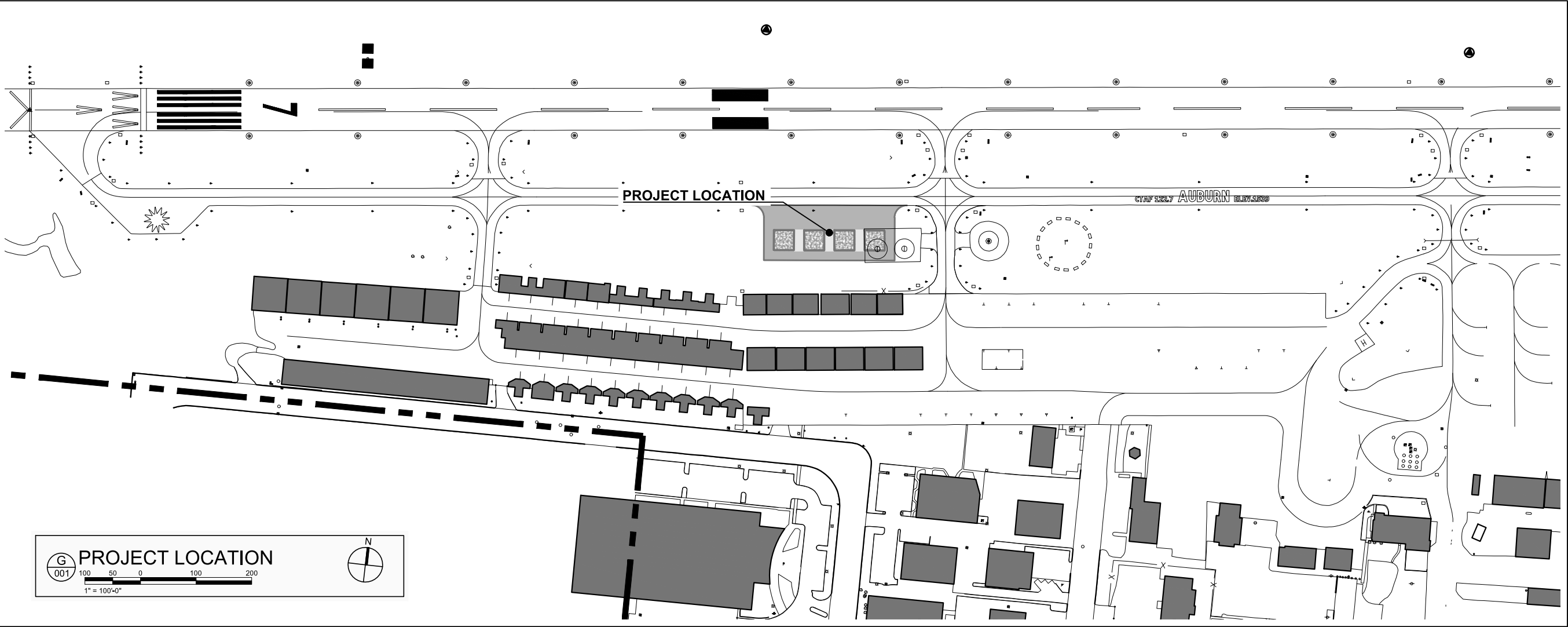
CONSTRUCTION PLANS FOR IMPROVEMENT TO  
**AUBURN MUNICIPAL AIRPORT**

AUBURN, CALIFORNIA

AIP PROJECT NUMBER 3-006-0012-021-2022

**SCHEDULE I**

HELICOPTER PARKING AREA PROJECT



Sponsored by:  
CITY OF AUBURN  
DEPARTMENT OF PUBLIC WORKS  
FEDERAL AVIATION ADMINISTRATION



Prepared for:  
AUBURN MUNICIPAL AIRPORT  
CITY OF AUBURN  
DEPARTMENT OF PUBLIC WORKS  
1225 LINCOLN WAY, ROOM 3  
AUBURN, CALIFORNIA 95603

**Jacobs**

3257 EAST GUASTI ROAD, SUITE 120  
ONTARIO, CALIFORNIA 91761  
(909) 974-2700

ISSUED FOR BID  
DATE: 02/24/22



CITY OF AUBURN



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ISSUE RECORD			
NO.	BY	DATE	DESCRIPTION

AIP PROJECT NUMBER: 3-06-0012-017	
JACOBS PROJECT NUMBER: W9Y24604	DATE: 11/25/19
DESIGNED: AW	DRAWN: AW
CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN  
**AUBURN  
MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER  
PARKING  
AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
**COVER**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>G001</b>
SHEET NUMBER <b>01</b> OF <b>21</b>	

GENERAL NOTES

1. ALL QUANTITIES ARE CONSIDERED APPROXIMATE ONLY.
2. THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT A WORK SCHEDULE AND PROPOSED CONSTRUCTION METHODS, CONSISTENT WITH THE DESIGNED PHASING PLAN, TEN (10) DAYS PRIOR TO THE BEGINNING OF WORK.
3. THE CONTRACTOR SHALL SURVEY AND COORDINATE PAVEMENT MARKING ALIGNMENTS AND OFFSETS AS REQUIRED ON THE PLANS. ALL SURVEY ACTIVITIES SHALL BE CONSIDERED INCIDENTAL TO ITEM C-105.
4. THE HORIZONTAL DATUM IS NAD 83. THE VERTICAL DATUM IS NAVD 88.
5. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION AND DEPTH OF ALL EXISTING UTILITIES WITHIN PROJECT LIMITS, STAGING, AND HAUL ROUTE AREAS PRIOR TO COMMENCING WORK. THE MAIN NUMBER FOR CALIFORNIA UNDERGROUND SERVICE ALERT NORTH IS 1-800-227-2600.
6. THE CONTRACTOR SHALL PROVIDE MATERIAL SUBMITTALS FOR THE ENGINEER'S APPROVAL PRIOR TO ORDERING.
7. THE CONTRACTOR SHALL MINIMIZE SITE DISTURBANCE, INCLUDING RUTS PRODUCED BY VEHICLES AND EQUIPMENT, WITHIN ALL CONSTRUCTION AREAS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES OR ROADS. REPAIRS SHALL BE MADE AT NO ADDITIONAL COST TO THE SPONSOR.
8. THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE ENGINEER PRIOR TO ESTABLISHING CONSTRUCTION ACCESS OR HAUL ROADS. EXISTING ROADS SHALL BE USED TO THE MAXIMUM EXTENT POSSIBLE.
9. CONSTRUCTION MANAGER/ SURVEYOR/ CONTRACTOR SHALL COORDINATE WITH THE ENGINEER TO IDENTIFY AND LOCATE EXISTING SITE CONTROL POINTS. THESE CONTROL POINTS SHALL BE THE BASIS FOR CONSTRUCTION LAYOUT AND STAKING, AS WELL AS THE AS-BUILT SURVEY OF ALL UTILITY ALIGNMENTS AND ALL APPURTENANCES. THE CONTRACTOR SHALL PROVIDE DIGITAL INFORMATION, IN THE FORM OF AN AUTOCAD FILE, TO THE ENGINEER.

PERMITS

1. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY FEDERAL, STATE OR LOCAL PERMITS REQUIRED FOR THE PROPOSED CONSTRUCTION.

QUANTITIES:

ITEM NO.	DESIGNATION	UNIT	QUANTITY
C-100-14.1	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	LS	1
C-102-5.1	EROSION CONTROL	LS	1
C-105-6.1	MOBILIZATION	LS	1
P-101-5.1	CONCRETE PAVEMENT REMOVAL	SY	50
P-101-5.2	ASPHALT PAVEMENT REMOVAL (COMPLETE)	SY	1,297
P-101-5.3	ASPHALT PAVEMENT REMOVAL (PARTIAL)	SY	280
L-101-5.4	REMOVE ELEVATED LIGHT AND FOUNDATION IN TURF (COMPLETE)	EA	2
P-101-5.5	REMOVE ELECTRICAL DUCT BANK	LF	360
P-152-4.1	UNCLASSIFIED EXCAVATION	CY	1,000
P-152-4.2	UNSUITABLE EXCAVATION	CY	165
P-152-4.3	SUBGRADE PREPARATION	SY	2,600
P-209-5.1	CRUSHED AGGREGATE BASE COURSE	CY	800
P-209-5.2	SEPARATION GEOTEXTILE	SY	2,600
P-401-8.1	BITUMINOUS PAVING COURSE	TON	650
P-401-8.2	MODIFIED BITUMINOUS MATERIAL (PG64-28)	TON	50
P-501-8.1	PORTLAND CEMENT CONCRETE PAVEMENT (6")	SY	580
P-603-5.1	EMULSIFIED ASPHALT TACK COAT	GAL	310
P-604-6.1	COMPRESSION JOINT SEALS FOR CONCRETE PAVEMENTS	LF	576
P-605-5.1	JOINT SEAL BETWEEN ASPHALT AND CONCRETE	LF	576
P-620-5.1	PERMANENT PAVEMENT MARKING	SF	8,254
P-620-5.2	TEMPORARY PAVEMENT MARKING	SF	8,254
F-162-5.1	INSTALL CHAIN LINK FENCE WITH SLATS	LF	173
T-901-5.1	SEEDING	AC	3
L-108-5.1	INSTALL #8 AWG, 5000V, L-824 TYPE C CABLE	LF	825
L-108-5.2	INSTALL #6 AWG BARE COPPER COUNTERPOISE	LF	260
L-108-5.1	INSTALL #2 AWG, 600V, XHHW-2	LF	783
L-108-5.2	INSTALL #4 AWG, 600V, XHHW-2	LF	456
L-110-5.1	INSTALL 1-WAY, 2-INCH PVC CONDUIT IN TURF (DEB)	LF	179
L-110-5.1	INSTALL 2-WAY, 2-INCH PVC CONDUIT IN TURF (DEB)	LF	152
L-110-5.2	INSTALL 1-WAY, 2-INCH PVC CONDUIT (CE)	LF	190
L-125-5.1	INSTALL L-861T(L), LED, TAXIWAY EDGE LIGHT ON NEW BASE, COMPLETE	EA	9
L-125-5.2	INSTALL L-852T(L), LED, IN-PAVEMENT TAXIWAY EDGE LIGHT ON NEW BASE, COMPLETE	EA	8
L-125-5.3	INSTALL L-853 RETROREFLECTIVE TAXIWAY EDGE MARKER – SURFACE MOUNT	EA	24
L-125-2.2	INSTALL DUPLEX CONVENIENCE RECEPTACLE WITH WEATHERPROOF COVER	EA	4
L-125-5.4	INSTALL L-858, SIZE 1, STYLE 4, STAKE MOUNTED GUIDANCE SIGN	EA	2

SHEET INDEX:

SHEET	DWG. NO	SHEET TITLE
01	G001	COVER SHEET
02	G002	INDEX OF DRAWINGS, SUMMARY OF APPROXIMATE QUANTITIES AND GENERAL NOTES
03	G003	CONSTRUCTION OPERATION AND SAFETY PLAN
04	G004	BORING LOG
05	G005	CONSTRUCTION STAGING AND PHASING PLAN
06	C101	EROSION CONTROL PLAN
07	C102	EROSION CONTROL DETAILS
08	C201	DEMOLITION PLAN
09	C301	GEOMETRIC PLAN
10	C302	GEOMETRIC PLAN - TAXIWAY D
11	C401	GRADING AND DRAINAGE PLAN
12	C501	PAVEMENT JOINT LAYOUT PLAN
13	C502	PAVEMENT JOINT DETAILS
14	C503	PAVEMENT SECTIONS
15	C504	PAINT MARKINGS PLAN
16	C505	GUIDANCE SIGN DETAILS
17	C506	FENCE DETAILS
18	E101	AIRFIELD LIGHTING PLAN
19	E102	LIGHTING DETAILS
20	E103	LIGHTING DETAILS
21	E103	LIGHTING DETAILS



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PROJECT TITLE:

HELICOPTER  
PARKING  
AREA

AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:

INDEX  
OF  
DRAWINGS

DATE  
24 FEBRUARY 2022

DRAWING NO.  
G002

SHEET NUMBER  
02 OF 21

AVIATION SAFETY REQUIREMENTS DURING CONSTRUCTION

1.

GENERAL SAFETY REQUIREMENTS

THROUGHOUT THE CONSTRUCTION PROJECT, THE FOLLOWING SAFETY AND OPERATIONAL PRACTICES SHOULD BE OBSERVED:

THE CONTRACTOR SHALL CONDUCT "TOOL BOX" SAFETY MEETINGS EACH WEEK, OPERATIONAL SAFETY SHOULD BE A STANDING AGENDA ITEM DURING PROGRESS MEETINGS THROUGHOUT THE CONSTRUCTION PROJECT.

THE CONTRACTOR AND ENGINEER MUST PERFORM ONSITE INSPECTIONS THROUGHOUT THE PROJECT, WITH IMMEDIATE REMEDY OF ANY DEFICIENCIES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.

AIRPORT RUNWAYS AND TAXIWAYS SHOULD REMAIN OPEN AT ALL TIMES UNLESS NOTED IN THE PHASING PLAN.

CONSTRUCTION THAT IS WITHIN THE SAFETY AREA OF AN ACTIVE RUNWAY, TAXIWAY, OR APRON THAT IS PERFORMED UNDER NORMAL OPERATIONAL CONDITIONS MUST BE PERFORMED WHEN THE RUNWAY, TAXIWAY, OR APRON IS CLOSED OR USE-RESTRICTED AND INITIATED ONLY WITH PRIOR PERMISSION FROM THE AIRPORT OPERATOR.

THE AIRPORT MANAGER MAY ORDER THE CONTRACTOR TO SUSPEND OPERATIONS IN THE EVENT OF AN EMERGENCY; MOVE PERSONNEL, EQUIPMENT, AND MATERIALS TO A SAFE LOCATION; BARRICADE ANY OPEN TRENCHES AND STAND-BY UNTIL AIRCRAFT USE IS COMPLETED.

2.

CONSTRUCTION MAINTENANCE AND FACILITIES MAINTENANCE

BEFORE BEGINNING ANY CONSTRUCTION ACTIVITY, THE CONTRACTOR MUST COORDINATE WITH THE ENGINEER THROUGH THE AIRPORT MANAGER, GIVE NOTICE, USING THE NOTICE TO AIRMEN (NOTAM) SYSTEM OF PROPOSED LOCATION, TIME, AND DATE OF COMMENCEMENT OF CONSTRUCTION. UPON COMPLETION OF WORK AND RETURN OF ALL SUCH AREAS TO STANDARD CONDITIONS, THE CONTRACTOR MUST, THROUGH THE AIRPORT MANAGER, VERIFY THE CANCELLATION OF ALL NOTICES ISSUED VIA THE NOTAM SYSTEM, FOR THE DURATION OF THE CONSTRUCTION PROJECT, THE CONTRACTOR MUST:

A. BE AWARE OF AND UNDERSTAND THE SAFETY PROBLEMS AND HAZARDS DESCRIBED IN AC 150/5370-2, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.

B. CONDUCT ACTIVITIES SO AS NOT TO VIOLATE ANY SAFETY STANDARDS CONTAINED IN AC 150/5370-2 OR ANY OF THE REFERENCES THEREIN.

C. INSPECT ALL CONSTRUCTION AND STORAGE AREAS AS OFTEN AS NECESSARY TO BE AWARE OF CONDITIONS, PROMPTLY TAKE ALL ACTIONS NECESSARY TO PREVENT OR REMEDY ANY UNSAFE OR POTENTIALLY UNSAFE CONDITIONS AS SOON AS THEY ARE DISCOVERED.

3.

RUNWAY AND TAXIWAY SAFETY AREA (RSA AND TSA)

CONSTRUCTION SAFETY INFORMATION

60' FROM CENTERLINE OF RUNWAY

125' FROM CENTERLINE OF RUNWAY

24.5' FROM CENTERLINE OF TAXIWAY

44.5' FROM CENTERLINE OF TAXIWAY

A. PROCEDURES FOR PROTECTING RUNWAY EDGES.

LIMIT CONSTRUCTION TO NO CLOSER THAN 250 FEET (60M) FROM THE RUNWAY CENTERLINE UNLESS THE RUNWAY IS CLOSED OR RESTRICTED TO AIRCRAFT OPERATIONS.

PREVENT PERSONNEL, MATERIAL, AND/OR EQUIPMENT, AS DEFINED IN AC 150/5300-13, PARAGRAPH 306, "OBSTACLE FREE ZONE (OFZ)," FROM PENETRATING THE OFZ.

B. PROCEDURES FOR PROTECTING RUNWAY ENDS.

LIMIT CONSTRUCTION TO NO CLOSER THAN 1000' FROM THE RUNWAY END AND 250' FROM THE RUNWAY CENTERLINE EXTENDED.

ENSURE ADEQUATE DISTANCE FOR BLAST PROTECTION IS PROVIDED, AS NEEDED.

4.

HAZARDOUS AREA MARKING AND LIGHTING

HAZARDOUS AREAS IN THE MOVEMENT AREA WILL BE MARKED WITH BARRICADES. THESE BARRICADES RESTRICT ACCESS AND MAKE HAZARDS OBVIOUS TO AIRCRAFT, PERSONNEL, AND VEHICLES, DURING PERIODS OF LOW VISIBILITY AND AT NIGHT. IDENTIFY HAZARDOUS AREAS WITH RED FLASHING LIGHTS. THE HAZARDOUS AREA MARKING AND LIGHTING WILL BE SUPPLIED BY THE CONTRACTOR, AS SPECIFIED IN THE CONTRACT.

5.

TEMPORARY LIGHTING AND MARKING

A. TEMPORARY LIGHTING FOR CONSTRUCTION SHALL INCLUDE SUFFICIENT PORTABLE LIGHT PLANTS TO BE USED AT ANY TIME DURING THE COURSE OF CONSTRUCTION WHEN OPERATIONS DICTATE WORK PRIOR TO DAWN AND AFTER DUSK. NO NIGHT TIME CONSTRUCTION WORK SHALL TAKE PLACE WITHOUT SUFFICIENT ARTIFICIAL LIGHTING TO MAINTAIN WORK QUALITY AND SAFETY AS APPROVED BY THE ENGINEER.

6.

VEHICLE OPERATION MARKING AND CONTROL

A. WHEN ANY VEHICLE, OTHER THAN ONE THAT HAS PRIOR APPROVAL FROM THE AIRPORT MANAGER, MUST TRAVEL OVER ANY PORTION OF AN AIRCRAFT MOVEMENT AREA, IT WILL BE PROPERLY IDENTIFIED, TO OPERATE IN THOSE AREAS DURING DAYLIGHT HOURS, THE VEHICLE MUST HAVE A FLAG OR BEACON ATTACHED TO IT. ANY VEHICLE OPERATING IN THE MOVEMENT AREAS DURING HOURS OF DARKNESS OR REDUCED VISIBILITY MUST BE EQUIPPED WITH AN AMBER FLASHING DOME-TYPE LIGHT.

7.

LIMITATIONS ON CONSTRUCTION

ADDITIONAL LIMITATIONS ON CONSTRUCTION INCLUDE:

A. OPEN-FLAME WELDING OR TORCH CUTTING OPERATIONS SHALL CONFORM TO LOCAL REQUIREMENTS AND RESTRICTIONS.

B. PROMINENTLY MARKING OPEN TRENCHES, EXCAVATIONS, STOCKPILED MATERIALS AND LIGHTING THESE OBSTACLES DURING HOURS OF RESTRICTED VISIBILITY AND DARKNESS.

C. MARKING AND LIGHTING CLOSED, DECEPTIVE, AND HAZARDOUS AREAS ON AIRPORTS AS APPROPRIATE.

D. CONSTRAINING STOCKPILED MATERIAL TO PREVENT ITS MOVEMENT AS A RESULT THE MAXIMUM ANTICIPATED AIRCRAFT BLAST AND FORECAST WIND CONDITIONS.

8.

RADIO COMMUNICATIONS

A. VEHICULAR TRAFFIC LOCATED IN OR CROSSING AN ACTIVE MOVEMENT AREA MUST HAVE WORKING TWO-WAY RADIO AND MONITOR THE COMMON TRAFFIC ADVISORY FREQUENCY (CTAF) (122.7 MHZ). THE DRIVER, THROUGH PERSONAL OBSERVATION, SHOULD CONFIRM THAT NO AIRCRAFT IS APPROACHING THE VEHICLE POSITION. CONSTRUCTION PERSONNEL MAY OPERATE IN A MOVEMENT AREA WITHOUT TWO-WAY RADIO COMMUNICATION PROVIDED, A NOTAM IS ISSUED CLOSING THE AREA AND THE AREA IS PROPERLY MARKED TO PREVENT INCURSIONS. TWO-WAY RADIO COMMUNICATIONS ARE REQUIRED BETWEEN CONTRACTORS AND THE AIRPORT MANAGER AND THE RESIDENT ENGINEER. CONTINUOUS MONITORING IS REQUIRED BY THE CONTRACTOR.

B. CONTRACTOR SHALL MAKE EVERY EFFORT TO NOT ENTER ANY ACTIVE SURFACE SAFETY AREA. SHOULD ACCESS BE NECESSARY TO ACTIVE SURFACE SAFETY AREAS, PRIOR PERMISSION OF THE AIRPORT MANAGER OR A DESIGNATED REPRESENTATIVE SHALL BE REQUIRED WITHIN 72 HOURS. COORDINATE REQUESTS FOR ACCESS THROUGH THE ENGINEER. FAILURE TO MEET THIS BASIC SAFETY REQUIREMENT COULD RESULT IN REMOVAL FROM THE SITE, PURSUANT TO THE GENERAL PROVISIONS OF THE CONTRACT.

9.

DEBRIS

WASTE AND LOOSE MATERIAL MUST NOT BE PLACED IN ACTIVE MOVEMENT AREAS. MATERIALS TRACKED ONTO THESE AREAS MUST BE REMOVED IMMEDIATELY DURING THE WORK PROJECT. CONTRACTOR SHALL HAVE A VACUUM SWEEPER ON SITE AT ALL TIMES DURING WORK ACTIVITIES.

THE CONTRACTOR IS ADVISED THAT DUST CONTROL, CLEANUP OF ACTIVE PAVEMENTS, TRACKING DEBRIS ONTO ACTIVE PAVEMENT AND GENERAL JOB SITE CLEANLINESS IS A SERIOUS SAFETY CONCERN DURING AIRPORT OPERATIONAL HOURS. FOD COULD CAUSE SERIOUS DAMAGE, INJURY OR DEATH THROUGH INGESTION IN MOVING AIRCRAFT. SPECIFIC ITEMS OF CONCERN INCLUDE BUT ARE NOT LIMITED TO: ANY PACKAGING FROM MATERIAL INSTALLATION, GRAVEL LEFT ON ACTIVE PAVEMENTS, DUST TRACKED ONTO ACTIVE PAVEMENTS, HAND TOOLS, HARDWARE DROPPED, ETC.

AIRCRAFT SECURITY

CONTRACTOR SHALL ADHERE TO AIRPORT SECURITY REQUIREMENTS AT ALL TIMES. ANY GATES UTILIZED FOR CONSTRUCTION PURPOSES SHALL BE GUARDED BY CONTRACTOR SUPPLIED PERSONNEL OR LOCKED AT ALL TIMES.

RUNWAY OBJECT FREE AREA/TAXIWAY OBJECT FREE AREA

NO EQUIPMENT, STOCKPILES ETC. MAY BE STORED WITHIN THE RUNWAY OR TAXIWAY OBJECT FREE AREA (OFA).

10.

AIRCRAFT RIGHT OF WAY

A. AIRCRAFT HAVE THE RIGHT-OF-WAY OVER CONSTRUCTION VEHICLES.

B. CONTRACTOR PERSONNEL WHO VIOLATE ANY AIRPORT SECURITY PROCEDURES ARE SUBJECT TO REMOVAL FROM THE PROJECT AT THE DISCRETION OF THE AIRPORT MANAGEMENT.

C. ANY FINES ISSUED TO THE AIRPORT FOR SECURITY VIOLATION SHALL BE PASSED ALONG TO THE CONTRACTOR AND BE FULLY REIMBURSED BY THE CONTRACTOR TO THE AIRPORT.

G1 TEMPORARY CONSTRUCTION MARKER (L-139) NTS

NOTES

1. FLASHER BARRICADES WILL BE REQUIRED ALONG THE EDGE OF EACH PHASED BOUNDARY. OPERATIONS WILL ISSUE NOTAM TO ADVISE AIRCRAFT OF THIS CONDITION.

2. BARRICADE SHALL BE SHERWIN INDUSTRIES (MODEL AR-10X96 HDPE) OR EQUAL. CONTRACTOR SHALL ORDER BARRICADE WITH REFLECTIVE SHEETING ON BOTH SIDES OF BARRICADE AND A COMPATIBLE SOLAR POWERED RED FLASHING LIGHT.

3. CONSTRUCTION MARKERS SHALL HAVE A MAXIMUM SPACING DISTANCE OF 20 FEET.

4. THE CONTRACTOR SHALL VERIFY THAT ALL CONSTRUCTION MARKERS ARE OPERATIONAL PRIOR TO LEAVING THE PROJECT SITE EACH NIGHT.

5. CONSTRUCTION MARKERS SHALL BE REMOVED ONLY AFTER A PHASE HAS BEEN COMPLETED AND ACCEPTED BY THE ENGINEER.

LEGEND

	AIRCRAFT PATH OF TRAVEL		EXISTING PULL BOX		PROPOSED FLOW LINE
	ASPHALT TO BE PARTIALLY REMOVED		EXISTING TAXIWAY LIGHT		PROPOSED GRADING EXTENTS/BOUNDARY
	ASPHALT TO BE REMOVED		INDICATES NUMBER OF CABLES IN CONDUIT FOR TAXIWAY CIRCUIT (1 CABLE IN CONDUIT UNLESS SHOWN)		PROPOSED GRADING - MAJOR CONTOUR
	BORE LOG LOCATION		PROPOSED ASPHALT SURFACE		PROPOSED GRADING - MINOR CONTOUR
	CONCRETE TO BE REMOVED		PROPOSED BUTT JOINT		PROPOSED GREEN PAINTED "NO TAXI" ISLAND
	CONTRACTOR PARKING/STAGING AREA		PROPOSED CHAIN LINK FENCE		PROPOSED SEEDING
	CONTRACTOR PATH OF TRAVEL		PROPOSED CONCRETE ENCASED CIRCUIT LIGHTING CABLE		PROPOSED SILT FENCE
	EROSION LOG		PROPOSED CONCRETE ENCASEMENT		PROPOSED TAIL ROTOR ARC (FOR INFORMATIONAL PURPOSES ONLY)
	EXISTING BUILDING		PROPOSED CONCRETE JOINT		PROPOSED TAXIWAY LIGHTING CABLE
	EXISTING CHAIN LINK FENCE		PROPOSED CONCRETE PAD		REINSTALL TAXIWAY LIGHT
	EXISTING CULVERT WITH FLARED END		PROPOSED CONSTRUCTION BARRIER		TAXIWAY CIRCUIT LIGHTING CABLE TO BE REMOVED (TO BE RE-USED)
	EXISTING ELECTRICAL DUCT		PROPOSED CULVERT WITH FLARED END		TAXIWAY LIGHT TO BE REMOVED (TO BE RE-USED)

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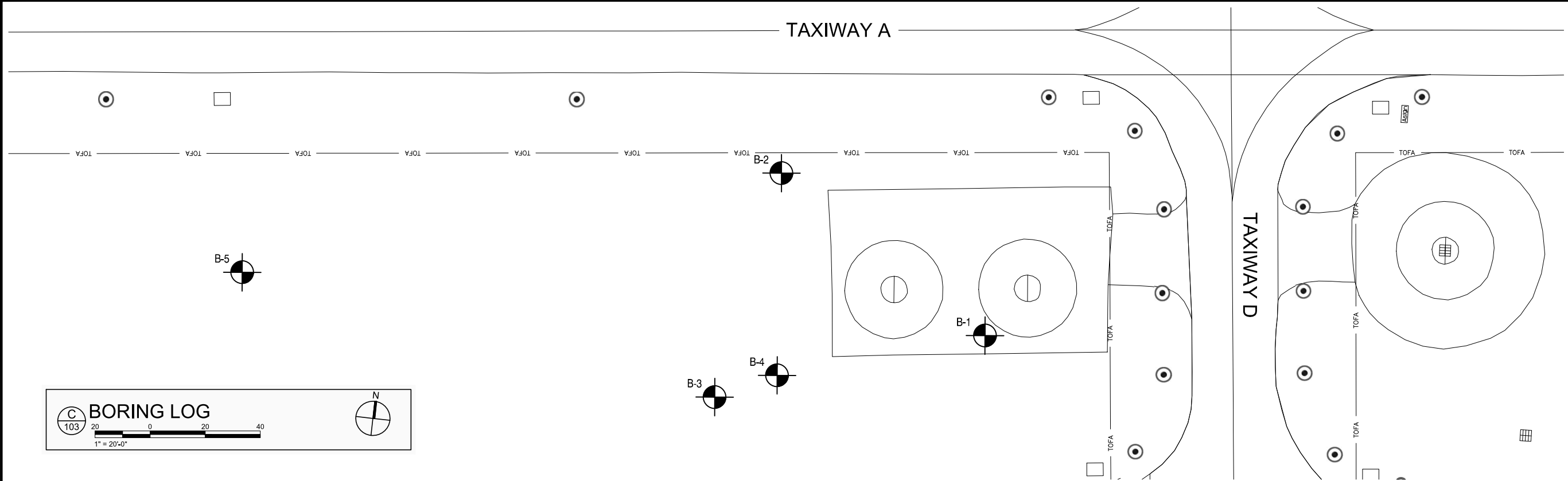
CITY OF AUBURN	
AUBURN MUNICIPAL AIRPORT	
Auburn, California, 95603 (530) 823-4211	
PLANNING AND PUBLIC WORKS	

PROJECT TITLE:	
HELICOPTER PARKING AREA	
AUBURN MUNICIPAL AIRPORT AUBURN, CALIFORNIA	

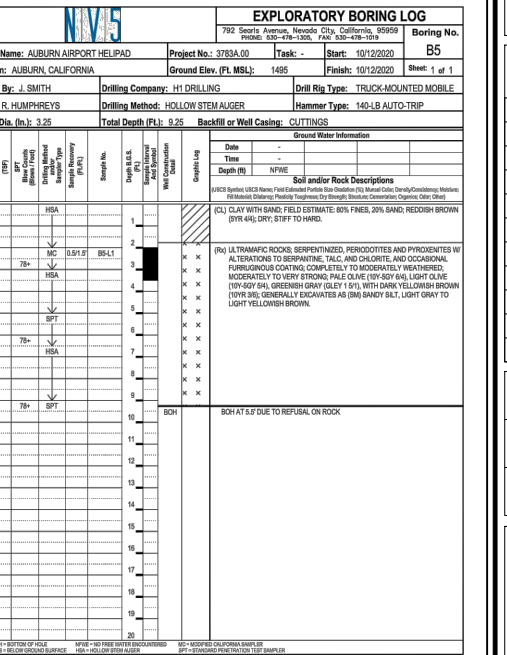
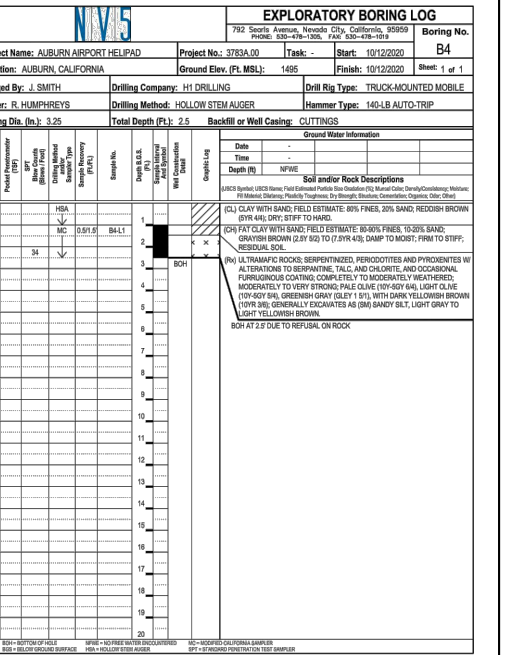
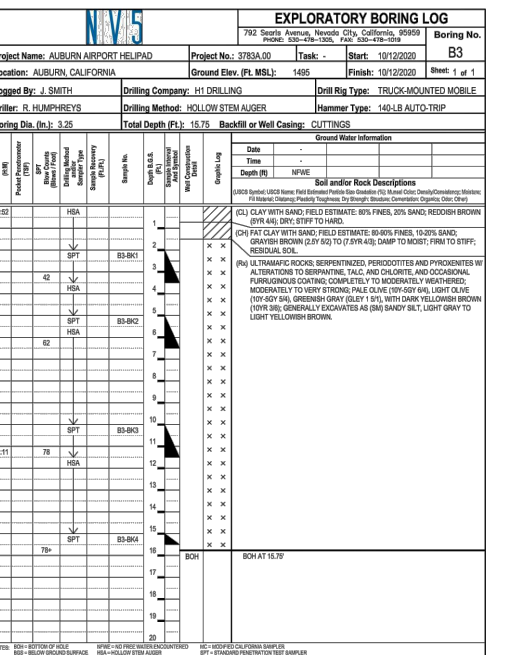
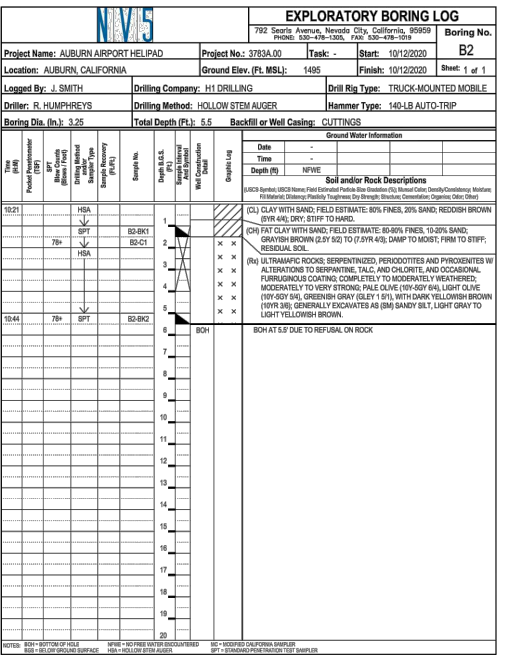
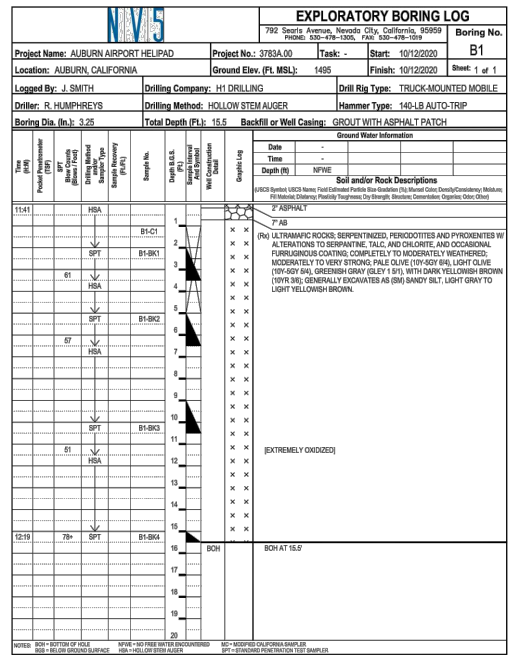
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SAFETY PLAN	
DATE 24 FEBRUARY 2022	DRAWING NO. G003
SHEET NUMBER 03 OF 18	

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Jacobs



**LEGEND**

 BORE LOG LOCATION

 EXISTING TAXIWAY LIGHTING

**NOTES**

- THE FINDINGS PRESENTED IN THIS REPORT ARE BASED ON NV5'S SUBSURFACE INVESTIGATION, LABORATORY TEST RESULTS, AND EXPERIENCE WITH SUBSURFACE CONDITIONS IN THE AREA.
- THE SOIL SURVEY INDICATES THAT THE PROJECT SITE IS LOCATED IN AN AREA CONTAINING AUBURN-ARGONAUT COMPLEX, 2 TO 5 PERCENT SLOPES. THE SOIL SURVEY DESCRIBES THE AUBURN AND ARGONAUT SOIL TYPES AS WELL DRAINED SOILS. THE AUBURN SOIL IS SHALLOW AND FORMED IN RESIDUUM FROM VERTICALLY TILTED BASIC SCHIST AND SLATE, THE ARGONAUT SOIL IS MODERATELY DEEP AND FORMED IN RESIDUUM FROM METABASIC ROCK.
- A TYPICAL PROFILE OF THE AUBURN SOIL TYPE IS DESCRIBED AS A SURFACE LAYER OF STRONG BROWN TO YELLOWISH RED SILT LOAM TO A DEPTH OF 20 INCHES BELOW THE GROUND SURFACE (BGS), UNDERLAIN BY PARTLY WEATHERED BASIC SCHIST.
- A TYPICAL PROFILE OF THE ARGONAUT SOIL TYPE IS DESCRIBED AS A SURFACE LAYER OF STRONG BROWN LOAM TO A DEPTH OF 4 INCHES BGS. THE LOAM IS TYPICALLY UNDERLAIN BY YELLOWISH RED SILT LOAM TO A DEPTH OF 9 INCHES BGS, THE SILT LOAM IS TYPICALLY UNDERLAIN BY YELLOWISH RED CLAY LOAM TO A DEPTH OF 16 INCHES BGS, A YELLOWISH BROWN WITH PATCHES OF YELLOWISH RED CLAY IS GENERALLY OBSERVED BELOW THE CLAY LOAM, TO A DEPTH OF 25 INCHES BGS. WEATHERED BASIC SCHIST TYPICALLY UNDERLIES THE CLAY.

**Note:**  
Information on this sheet is derived from the Geotechnical Engineering Report dated November 13, 2020 completed by NV5. It is shown for information only. A sealed copy of the Geotechnical Report is included with Contract Documents and Specifications for this project.

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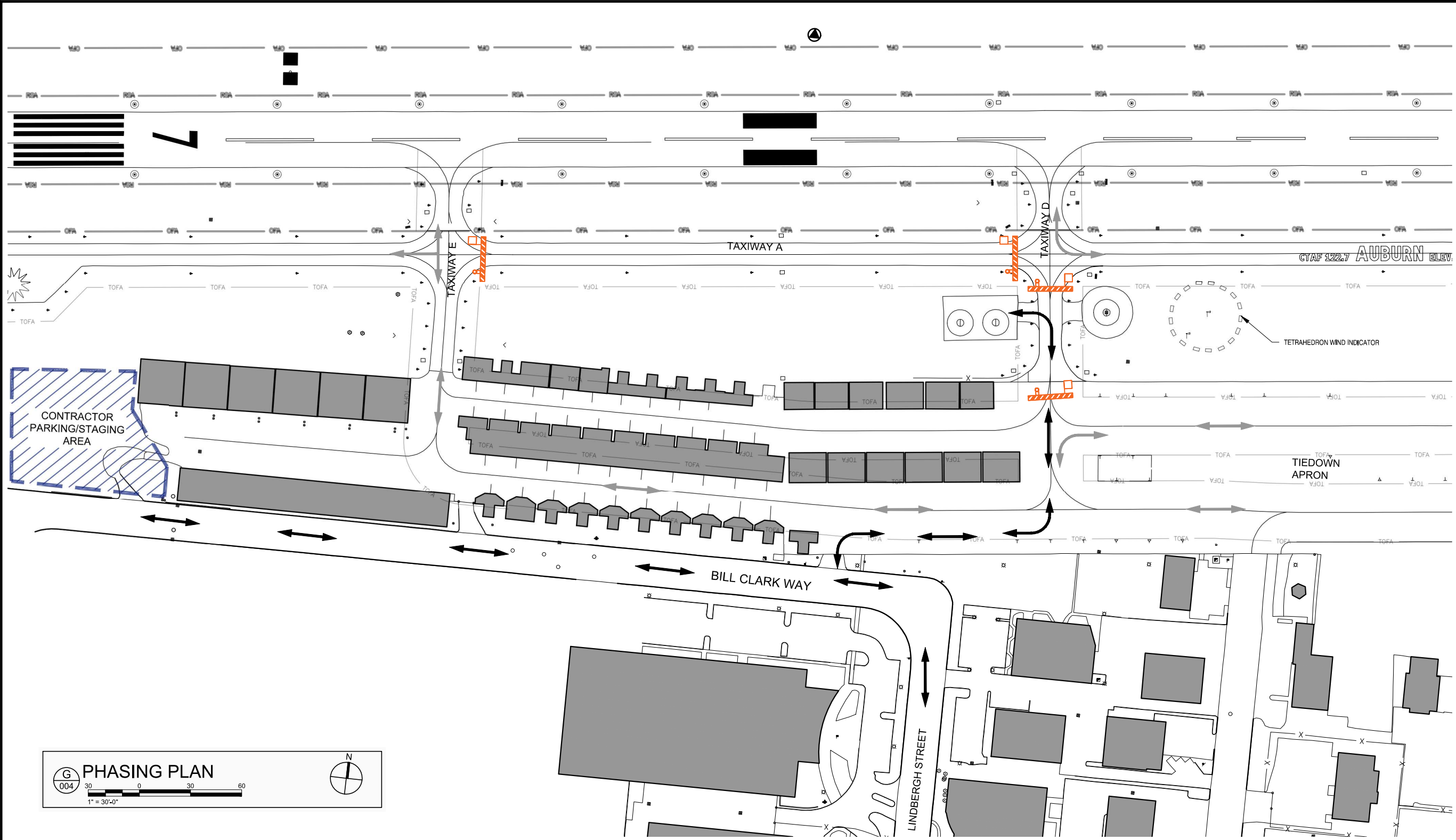
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AUBURN, CALIFORNIA

DRAWING TITLE:  
**BORING LOG**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>G004</b>
SHEET NUMBER 04 OF 21	





Jacobs



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DRAWING TITLE:  
**PHASING PLAN**

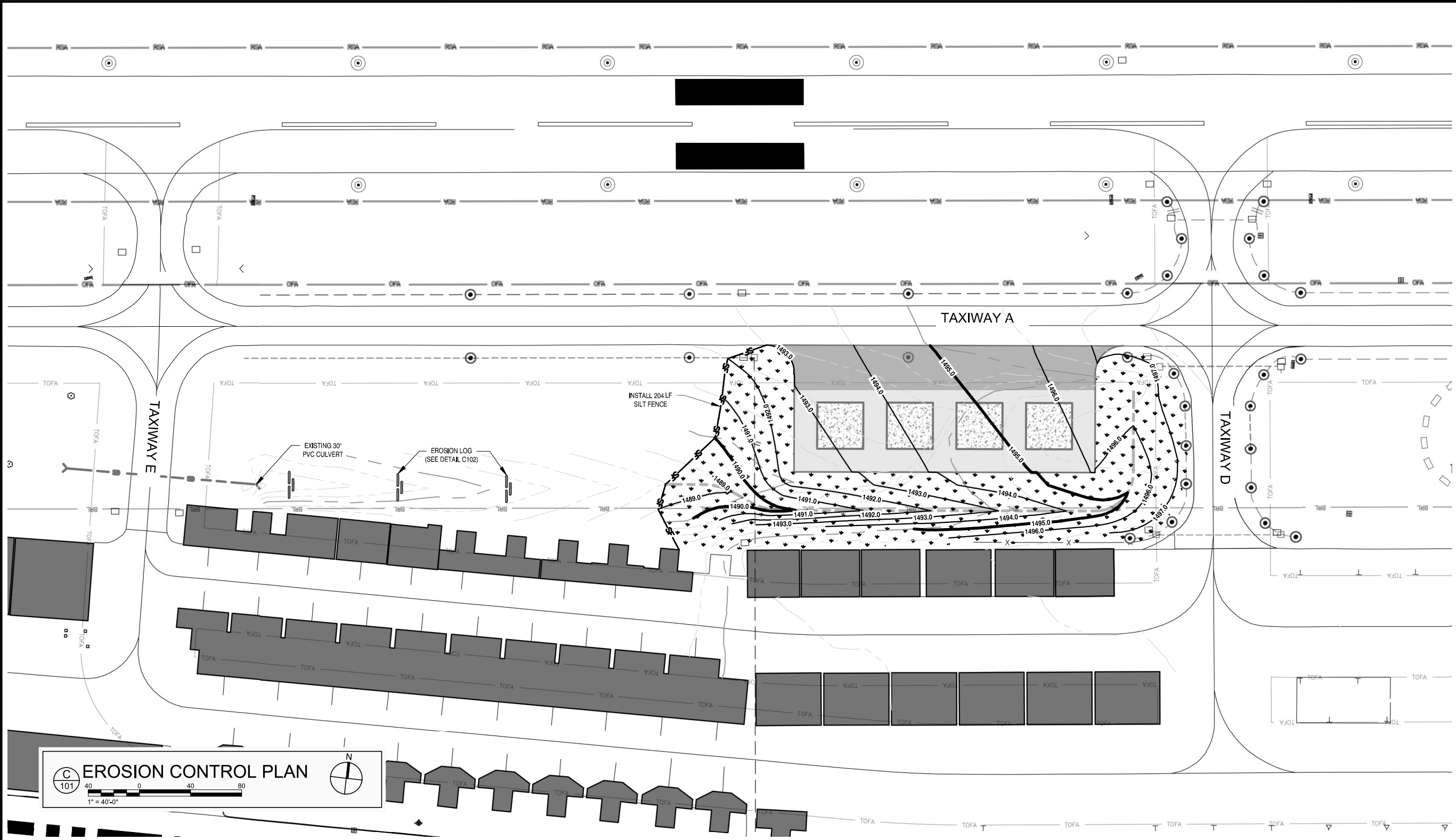
DATE 24 FEBRUARY 2022	DRAWING NO. <b>G005</b>
SHEET NUMBER 05 OF 21	

**LEGEND**

- AIRCRAFT PATH OF TRAVEL
- TEMPORARY CONSTRUCTION MARKER
- CONTRACTOR PARKING/STAGING AREA
- CONTRACTOR PATH OF TRAVEL

CONSTRUCTION PHASING NOTES				
SCHEDULE I				
PHASE	SCOPE OF WORK	DURATION	AIRPORT OPERATIONAL NOTES	COMMENTS
1A	<ul style="list-style-type: none"><li>Set Out Construction Barricades</li><li>Demolition</li><li>Excavation</li><li>Subgrade Preparation</li></ul>	30 CALENDAR DAYS	<ul style="list-style-type: none"><li>Runway will be operational at all times.</li><li>Maintain ARFF access at all times.</li><li>Taxiway A closure between Taxiway D and Taxiway E.</li><li>Taxiway D closure between Apron and Taxiway A.</li></ul>	<ul style="list-style-type: none"><li>Airport manager to issue NOTAM for closure of Taxiway D and A.</li><li>De-energize taxiway lighting</li><li>Contractor operations are restricted to closed work areas</li><li>Construction barricades shall be installed per phasing plans and as required by AC-150/5370-2 (latest edition) and as directed by AUN.</li></ul>
1B	<ul style="list-style-type: none"><li>Aggregate Base</li><li>Drainage</li><li>Bituminous Paving</li><li>Pavement Striping</li><li>Airfield Electrical</li></ul>	25 CALENDAR DAYS	<ul style="list-style-type: none"><li>Runway will be operational at all times.</li><li>Maintain ARFF access at all times.</li><li>Taxiway A closure between Taxiway D and Taxiway E.</li><li>Taxiway D closure between Apron and Taxiway A.</li><li>Complete work from taxiway pavement edge to Taxiway Object Free Area.</li><li>Taxiway A and D open at completion</li></ul>	<ul style="list-style-type: none"><li>Contractor operations are restricted to closed work areas.</li><li>All work areas to be inspected prior to reopening to aircraft operations.</li><li>Re-energize taxiway lighting.</li><li>Contractor to remove barricades</li></ul>
1B	<ul style="list-style-type: none"><li>Aggregate Base</li><li>Concrete Paving</li><li>Bituminous Paving</li><li>Pavement Striping</li><li>Seeding</li></ul>	35 CALENDAR DAYS	<ul style="list-style-type: none"><li>Runway will be operational at all times.</li><li>Maintain ARFF access at all times.</li><li>Taxiway A and D are operational</li></ul>	<ul style="list-style-type: none"><li>Contractor operations are restricted to closed work areas.</li><li>All work areas to be inspected prior to reopening to aircraft operations.</li></ul>

- NOTES**
- CONTRACTOR TO COORDINATE TEMPORARY HELICOPTER PARKING WITH THE AIRPORT.
  - FLAGGER NEEDED AT TAXIWAY D.
  - INSTALL TEMPORARY CONSTRUCTION MARKER, SEE SHEET G003, DETAIL G1.



LEGEND

EROSION LOG

EXISTING BUILDING

EXISTING CHAIN LINK FENCE

EXISTING CULVERT WITH FLARED END

PROPOSED ASPHALT SURFACE

PROPOSED CONCRETE PAD

PROPOSED SEEDING

PROPOSED SILT FENCE

NOTES

- ALL GRADING SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF AUBURN BUILDING CODE.
- THE CONTRACTOR SHALL OBTAIN ANY NECESSARY CITY AND BASE PERMITS PRIOR TO STARTING WORK.
- NO GRADING WORK SHALL BE PERFORMED AT ANY TIME WITHOUT AN EROSION CONTROL SYSTEM APPROVED BY THE CITY OF AUBURN.
- THE CONTRACTOR SHALL PROVIDE EROSION AND SEDIMENTATION CONTROL. USE RECOMMENDATIONS BY THE BEST MANAGEMENT PRACTICES (BMPs) TO IMPLEMENT CONTROL.
- WHERE NO SLOPE GRADIENT IS SHOWN, PROVIDE STRAIGHT GRADE BETWEEN CONTOURS.

CITY OF AUBURN

James Mills  
REGISTERED PROFESSIONAL ENGINEER  
C 68832  
F CALIFORNIA

Jacobs

Know what's below.  
Call before you dig.

ISSUE RECORD

NO.	BY	DATE	DESCRIPTION

AIP PROJECT NUMBER:  
3-06-0012-017

JACOBS PROJECT NUMBER:  
W9Y24604

DATE:  
11/25/19

DESIGNED:  
AW

DRAWN:  
AW

CHECKED:  
JLW

APPROVED:  
JJM

CITY OF AUBURN

AUBURN  
MUNICIPAL AIRPORT  
Auburn, California, 95603  
(530) 823-4211

PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
HELICOPTER  
PARKING  
AREA  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
EROSION CONTROL  
PLAN

DATE  
24 FEBRUARY 2022

SHEET NUMBER  
06 OF 21

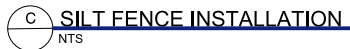
DRAWING NO.  
C101

- THE REQUIRED MINIMUM PERMANENT STORMWATER MANAGEMENT PRACTICES ARE SHOWN AND NOTED ON THE DRAWINGS. SOME TEMPORARY MEASURES MAY ALSO BE SHOWN AS NECESSARY TO SATISFY STORMWATER PLANS AND LOCAL PERMITTING REQUIREMENTS.
- CONTRACTOR SHALL OBTAIN A STORMWATER MANAGEMENT PERMIT AS REQUIRED BY THE STATE OF CALIFORNIA. CONTRACTOR MAY SUPPLEMENT THE INFORMATION PROVIDED ON THESE DRAWINGS AND SPECIFICATIONS AS NECESSARY FOR THE PREPARATION OF A STORMWATER MANAGEMENT PLAN TO OBTAIN SUCH PERMIT. THE CIVIL ENGINEER SHALL HAVE NO DUTY OR RESPONSIBILITY WITH RESPECT TO PREPARATION OF THIS PLAN.
- ALL DISTURBED AREAS NOT RECEIVING FINISHED GRADE OR LANDSCAPE TREATMENT SHALL RECEIVE TOP SOILS REPLACEMENT AND NATIVE GRASS RE-VEGETATION AS A MINIMUM. RE-VEGETATION SHALL OCCUR AS SOON AS FEASIBLE. SEE EROSION CONTROL DETAILS THIS SHEET.
- CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE VEGETATION GROWS IN ALL DISTURBED, RE-SEEDING AREAS, TO THE SATISFACTION OF THE CONTRACTING OFFICER. SEE SPECIFICATION T-901.
- ALL EROSION CONTROL MEASURES SHALL BE INSTALLED AS EARLY AS PRACTICAL. ALL CONTROLS SHALL BE MONITORED REGULARLY, AND MODIFIED TO MAINTAIN EFFECTIVENESS.
- THE ABOVE MEASURES AND PROCEDURES SHALL BE IMPLEMENTED AND COMPLETED AS EACH PHASE PROGRESSES BEFORE PROCEEDING ON TO THE NEXT PHASE.
- DUST PALLIATIVES WILL BE APPLIED TO ALL GRAVEL/SOIL CONSTRUCTION HAUL ROADS TO REDUCE DUST AND PREVENT SOIL PARTICLES FROM BECOMING AIRBORNE. DUST PALLIATIVES USED IN TRAFFIC AREAS MUST WITHSTAND ABRASION AND RUTS CREATED AS A RESULT OF CONSTRUCTION TRAFFIC. RUTS SHOULD BE LIMITED TO 1/4 INCH OTHERWISE THE TRAFFIC SURFACE SHALL BE RE-TREATED.
- EXACT LOCATION AND SIZE OF TOPSOIL STOCKPILE AREAS WILL BE COORDINATED AND APPROVED BY THE CONTRACTING OFFICER.
- ENSURE A LIST OF ALL DUST PALLIATIVES THAT ARE GOING TO BE UTILIZED PROVIDED TO CONTRACTING OFFICER FOR EVALUATION AND ACCEPTANCE AT LEAST 14 DAYS PLANNED APPLICATION DATE.
- EROSION SILTATION LOGS SHALL BE PLACED ACROSS THE WIDTH OF ALL DRAINAGE SWALES AT A MINIMUM FREQUENCY OF 1 PER 100 LINER FEET.



1. SEDIMENT CONTROL LOGS SHALL CONSIST OF STRAW, COMPOST, EXCELSIOR OR COCONUT FIBER, AND SHALL BE FREE OF ANY NOXIOUS WEEDS, SEEDS OR DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
2. SEDIMENT CONTROL LOGS MAY BE USED AS SMALL CHECK DAMS IN DITCHES AND SWALES. HOWEVER, THEY SHOULD NOT BE USED IN HIGH VELOCITY DRAINAGE WAYS.
3. THE UPHILL SIDE OF THE SEDIMENT CONTROL LOG SHALL BE BACKFILLED WITH SOIL THAT IS FREE OF ROCKS AND DEBRIS. THE SOIL SHALL BE TIGHTLY COMPACTED INTO THE SHAPE OF A RIGHT TRIANGLE USING A SHOVEL OR WEIGHTED LAWN ROLLER.
4. SEDIMENT CONTROL LOGS SHALL BE TIGHTLY ABUTTED AT JOINTS WITH NO GAPS. OVERLAP SEDIMENT CONTROL LOGS A MINIMUM OF 12" AT JOINTS.

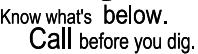
1. THE SWAMP MANAGER SHALL INSPECT SEDIMENT CONTROL LOGS WEEKLY, DURING AND AFTER ANY STORM EVENT AND MAKE REPAIRS AS NEEDED.
2. SEDIMENT ACCUMULATED UPSTREAM OF THE SEDIMENT CONTROL LOG SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS  $\frac{1}{2}$  OF THE HEIGHT OF THE SEDIMENT CONTROL LOG.
3. SEDIMENT CONTROL LOG SHALL BE REMOVED AT THE END OF CONSTRUCTION. IF DISTURBED AREAS EXIST AFTER REMOVAL THEY SHALL BE COVERED WITH TOP SOIL, SEEDED AND MULCHED, OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE CONTRACTING OFFICER.



## C2 SILT FENCE DETAIL

1. SILT FENCE MUST BE PLACED AWAY FROM THE TOE OF THE SLOPE TO ALLOW FOR WATER PONDING. SILT FENCE AT THE TOE OF A SLOPE SHOULD BE INSTALLED IN A FLAT LOCATION AT LEAST 2 FEET FROM THE TOE OF THE SLOPE TO ALLOW ROOM FOR PONDING AND DEPOSITION.
2. A UNIFORM ANCHOR TRENCH SHALL BE EXCAVATED USING TRENCHER OR SILT FENCE INSTALLATION DEVICE. NO ROAD GRADERS, BACKHOES, OR SIMILAR EQUIPMENT SHALL BE USED.
3. COMPACT ANCHOR TRENCH BY HAND WITH A JUMPING JACK OR BY WHEEL ROLLING. COMPACTION SHALL BE SUCH THAT SILT FENCE RESISTS BEING PULLED OUT OF ANCHOR TRENCH BY HAND.
4. SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD BE NO NOTICEABLE SAG BETWEEN STAKES AFTER IT HAS BEEN ANCHORED TO THE STAKES.
5. SILT FENCE FABRIC SHALL BE MIRAFI 100 X OR APPROVED EQUAL. FABRIC SHALL BE ANCHORED TO THE STAKES USING 1" HEAVY DUTY STAPLES OR NAILS WITH 1" HEADS. STAPLES AND NAILS SHOULD BE PLACED 3' ALONG THE FABRIC DOWN THE STAKE.
6. AT THE END OF A RUN OF SILT FENCE ALONG A CONTOUR, THE SILT FENCE SHOULD BE TURNED PERPENDICULAR TO THE CONTOUR TO CREATE A "J-HOOK". THE "J-HOOK" EXTENDING PERPENDICULAR TO THE CONTOUR SHOULD BE OF SUFFICIENT LENGTH TO KEEP RUNOFF FROM FLOWING AROUND THE END OF THE SILT FENCE (TYPICALLY 10' - 20').
7. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.

1. THE SWMP MANAGER SHALL INSPECT SILT FENCE WEEKLY, DURING AND AFTER ANY STORM EVENT AND MAKE REPAIRS OR CLEAN OUT AS NEEDED.
2. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL OF GEOTEXTILE FABRIC TO AVOID JOINTS. WHEN JOINTS ARE NECESSARY, MATERIAL SHALL BE OVERLAPPED TO THE NEXT POST OR THE ADJOINING FABRICS WRAPPED TOGETHER AROUND POSTS.
3. SEDIMENT ACCUMULATED UPSTREAM OF SILT FENCE SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS  $\frac{1}{2}$  THE HEIGHT OF THE FENCE OR SOONER IF THE FENCE REQUIRES REPAIR.
4. WHEN SILT FENCE IS REMOVED, THE DISTURBED AREA SHALL BE COVERED WITH TOP SOIL, DRILL SEEDS AND CRIMP MULCHED, OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE CONTRACTING OFFICER.

[illegible]

AIP PROJECT NUMBER: 3-06-0012-017			
JACOBS PROJECT NUMBER: W9Y24604			DATE: 11/25/19
DESIGNED: AW	DRAWN: AW	CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN

**AUBURN  
MUNICIPAL AIRPORT**

Auburn, California, 95603  
(530) 823-4211

PLANNING AND PUBLIC WORKS

PROJECT TITLE:

**HELICOPTER  
PARKING  
AREA**

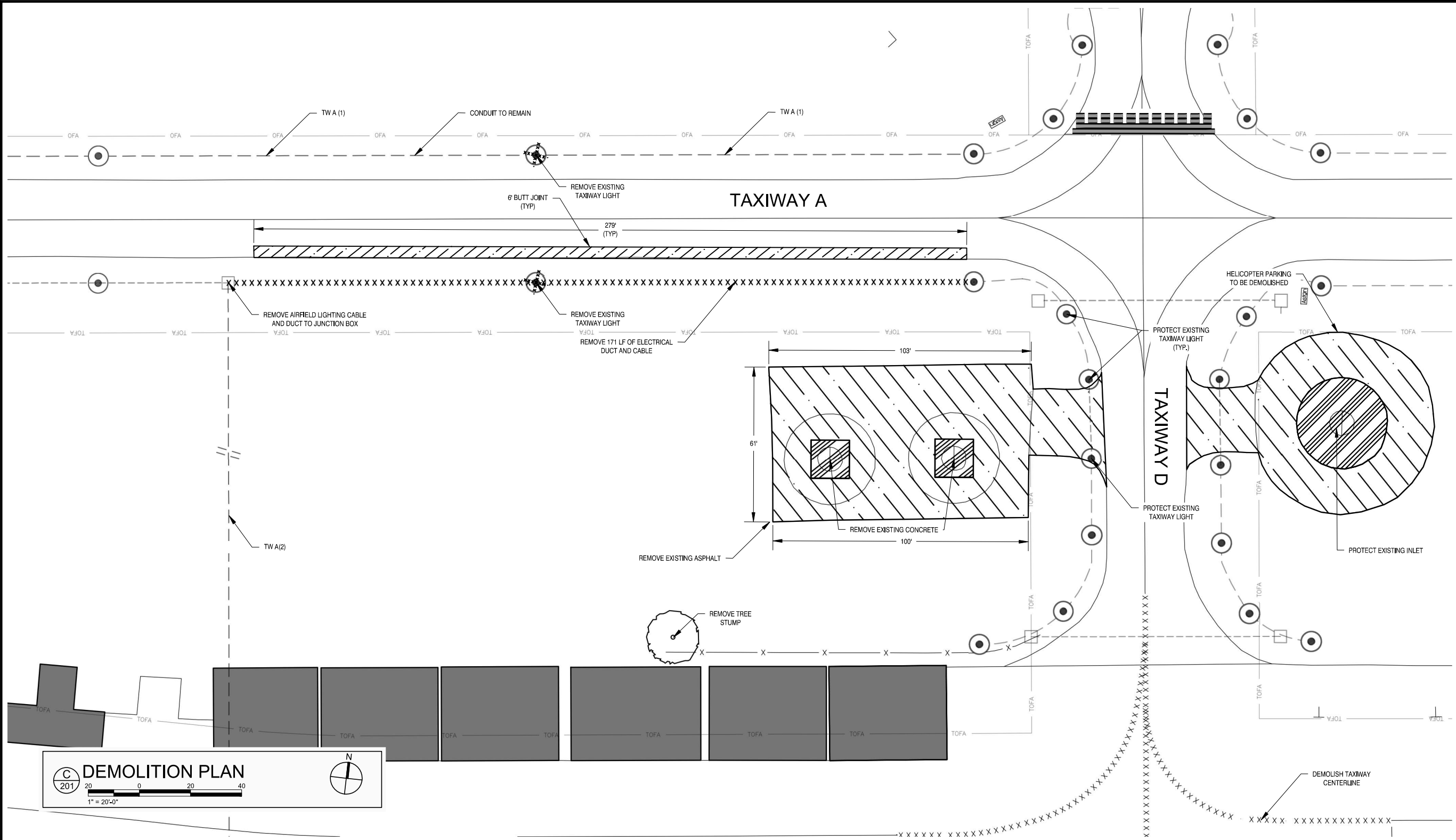
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:

EROSION CONTROL  
DETAILS

DATE 24 FEBRUARY 2022	DRAWING NO.  C102
SHEET NUMBER 07 OF 21	





C201

DEMOLITION PLAN

20 0 20 40

1" = 20'-0"

N

LEGEND

	ASPHALT TO BE PARTIALLY REMOVED		EXISTING TAXIWAY LIGHTING
	ASPHALT TO BE REMOVED		INDICATES NUMBER OF CABLES IN CONDUIT FOR TAXIWAY CIRCUIT (1 CABLE IN CONDUIT UNLESS SHOWN)
	CONCRETE TO BE REMOVED		TAXIWAY CIRCUIT LIGHTING CABLE TO BE REMOVED
	EXISTING BUILDING		REMOVE TAXIWAY LIGHT AND FOUNDATION. SALVAGE LIGHT AND TRANSFORMER FOR REINSTALLATION.
	EXISTING CHAIN LINK FENCE		
	EXISTING ELECTRICAL DUCT		
	EXISTING PULL BOX		

NOTES

- ANY PAVEMENT DAMAGED DURING REMOVAL OUTSIDE THE PROPOSED REMOVAL LIMITS SHALL BE SQUARED OFF TO THE SATISFACTION OF THE ENGINEER AND ALL COSTS ASSOCIATED WITH THE ADDITIONAL REMOVAL AND RECONSTRUCTION SHALL BE BORNE BY THE CONTRACTOR.
- CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES. IF FIELD OBSERVATIONS INDICATED CONDITIONS DIFFERENT THAN SHOWN ON THE PLANS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IN CASE OBSERVED UTILITY LOCATIONS CONFLICT WITH PROPOSED CONSTRUCTION.
- THE CONTRACTOR SHALL REPAIR ANY UTILITIES DAMAGED DURING DEMOLITION AT NO ADDITIONAL EXPENSE TO THE AIRPORT.
- CONTRACTOR SHALL USE EXTREME CAUTION WHEN EXCAVATING ADJACENT TO EXISTING STRUCTURES. IF UNDERMINING OF THE BASE COURSE MATERIAL OF THE EXISTING STRUCTURES OCCURS THE CONTRACTOR SHALL HALT EXCAVATION AND PROPOSE A SOLUTION TO THE ENGINEER TO RECTIFY AND STOP THE UNDERMINING AND PREVENT DAMAGE TO THE EXISTING STRUCTURES.
- REMOVE TIE POINTS AND/OR GROUNDING RODS. THIS WORK WILL BE INCLUDED IN THE PAVEMENT DEMOLITION.
- PAINT MARKING DEMOLITION LIMITS WILL BE BASED OFF OF NEW MARKING LIMITS AS SHOWN ON SHEET C302.



CITY OF AUBURN



James Mills

Jacobs



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ISSUE RECORD

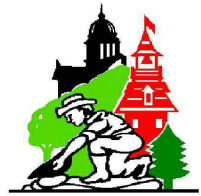
NO.	BY	DATE	DESCRIPTION

AIP PROJECT NUMBER: 3-06-0012-017			
JACOBS PROJECT NUMBER: W9Y24604		DATE: 11/25/19	
DESIGNED: AW	DRAWN: AW	CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN  
**AUBURN MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER PARKING AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE: <b>DEMOLITION PLAN</b>	
DATE 24 FEBRUARY 2022	DRAWING NO. <b>C201</b>
SHEET NUMBER 08 OF 21	



CITY OF AUBURN



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NO.	BY	DATE	DESCRIPTION

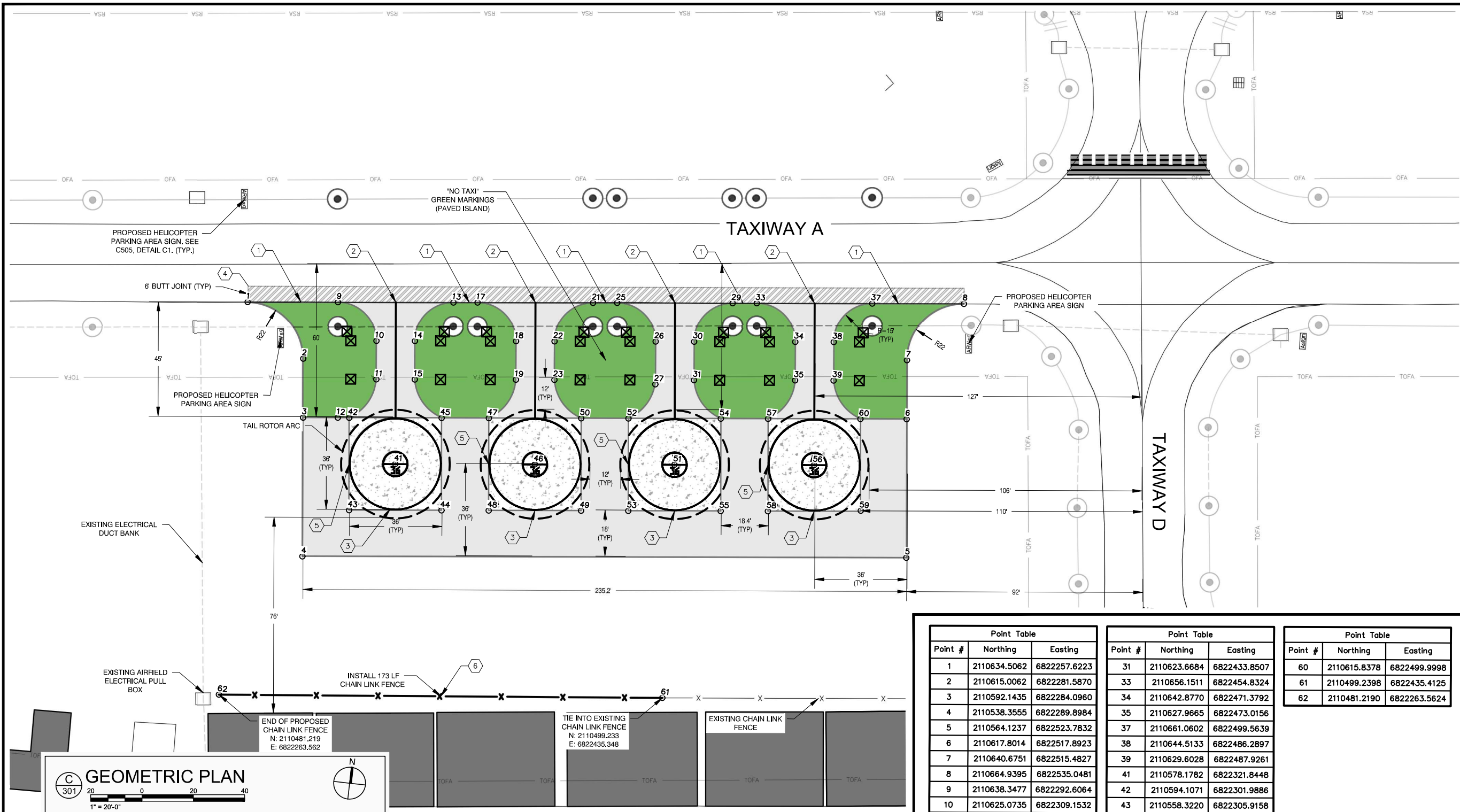
AIP PROJECT NUMBER: 3-06-0012-017	
JACOBS PROJECT NUMBER: W9Y24604	DATE: 11/25/19
DESIGNED: AW	DRAWN: AW
CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN  
**AUBURN  
MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER  
PARKING  
AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
**GEOMETRIC  
PLAN**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>C301</b>
SHEET NUMBER 09 OF 21	



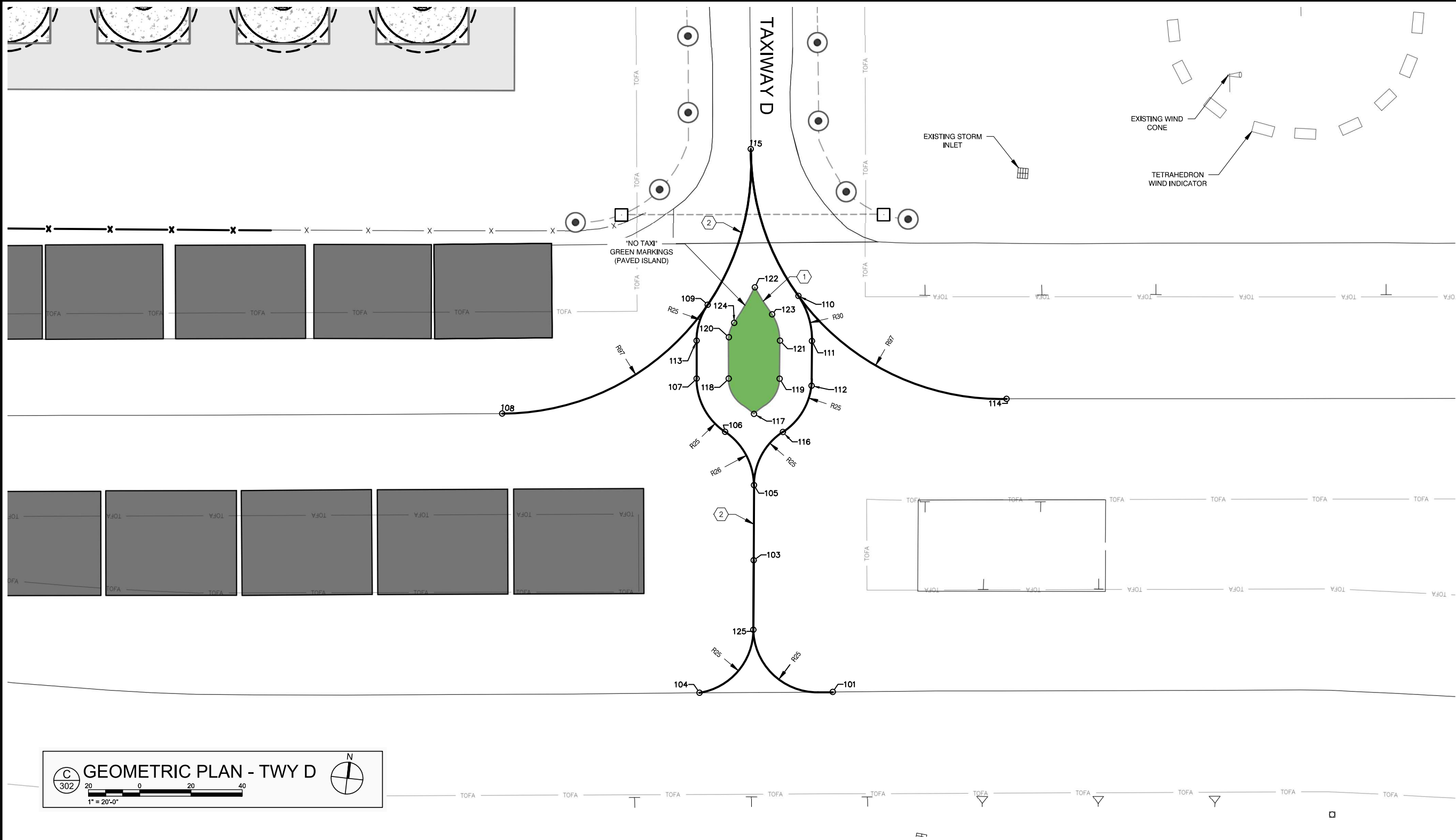
LEGEND			
	EXISTING BUILDING		PROPOSED CHAIN LINK FENCE
	EXISTING CHAIN LINK FENCE		PROPOSED CONCRETE PAD
	EXISTING ELECTRICAL DUCT		PROPOSED GREEN PAINTED "NO TAXI" ISLAND
	EXISTING PULL BOX		PROPOSED TAIL ROTOR ARC (FOR INFORMATIONAL PURPOSES ONLY)
	EXISTING TAXIWAY LIGHT		PROPOSED TAXIWAY EDGE REFLECTOR
	PROPOSED ASPHALT SURFACE		PROPOSED TAXIWAY EDGE LIGHT
	PROPOSED BUTT JOINT		

NOTES	
1	INSTALL GREEN PAINTED "NO TAXI" ISLAND WITH BORDER (PAVED). SEE SHEET C504, DETAIL C3.
2	INSTALL 6" WIDE YELLOW LINE MARKING. SEE SHEET C504, DETAIL C4. SEE SHEET C504 FOR EXACT MARKING LOCATIONS.
3	INSTALL HELICOPTER PARKING MARKINGS. SEE SHEET C504, DETAIL C1.
4	INSTALL BUTT JOINT. SEE SHEET C305, DETAIL E.
5	CONCRETE JOINT PLAN. SEE SHEET C501, DETAIL C1. FOR JOINT DETAILS SEE SHEET C502.
6	INSTALL CHAIN LINK FENCE. SEE SHEET C506 FOR FENCE DETAILS.

Point Table		
Point #	Northing	Easting
1	2110634.5062	6822257.6223
2	2110615.0062	6822281.5870
3	2110592.1435	6822284.0960
4	2110538.3555	6822289.8984
5	2110564.1237	6822523.7832
6	2110617.8014	6822517.8923
7	2110640.6751	6822515.4827
8	2110664.9395	6822535.0481
9	2110638.3477	6822292.6064
10	2110625.0735	6822309.1532
11	2110610.1630	6822310.7896
12	2110593.6162	6822297.5155
13	2110643.2567	6822337.3378
14	2110626.7099	6822324.0637
15	2110611.7994	6822325.7001
17	2110644.2821	6822346.6817
18	2110631.0080	6822363.2286
19	2110616.0975	6822364.8649
21	2110649.1912	6822391.4131
22	2110632.6444	6822378.1391
23	2110617.7339	6822379.7754
25	2110650.2166	6822400.7571
26	2110636.9425	6822417.3039
27	2110620.3957	6822419.0298
29	2110655.1257	6822445.4885
30	2110638.5788	6822432.2144

Point Table		
Point #	Northing	Easting
31	2110623.6684	6822433.8507
33	2110656.1511	6822454.8324
34	2110642.8770	6822471.3792
35	2110627.9665	6822473.0156
37	2110661.0602	6822499.5639
38	2110644.5133	6822486.2897
39	2110629.6028	6822487.9261
41	2110578.1782	6822321.8448
42	2110594.1071	6822301.9866
43	2110558.3220	6822305.9158
44	2110562.2492	6822341.7010
45	2110598.0344	6822337.7737
46	2110584.1127	6822375.9201
47	2110600.0416	6822356.0639
48	2110564.2565	6822359.9912
49	2110568.1837	6822395.7763
50	2110603.9688	6822391.8491
51	2110590.0471	6822429.9955
52	2110605.9761	6822410.1393
53	2110570.1909	6822414.0665
54	2110609.9033	6822445.9244
55	2110574.1182	6822449.8516
56	2110595.9816	6822484.0708
57	2110611.9106	6822464.2146
58	2110576.1254	6822468.1418
59	2110580.3107	6822504.1049

Point Table		
Point #	Northing	Easting
60	2110615.8378	6822499.9998
61	2110499.2398	6822435.4125
62	2110481.2190	6822263.5624



LEGEND

	EXISTING BUILDING		PROPOSED CHAIN LINK FENCE
	EXISTING CHAIN LINK FENCE		PROPOSED CONCRETE PAD
	EXISTING ELECTRICAL DUCT		PROPOSED GREEN PAINTED 'NO TAXI' ISLAND
	EXISTING PULL BOX		PROPOSED TAIL ROTOR ARC (FOR INFORMATIONAL PURPOSES ONLY)
	EXISTING TAXIWAY LIGHT		
	PROPOSED ASPHALT SURFACE		
	PROPOSED BUTT JOINT		

POINT TABLES

Point Table		
Point #	Northing	Easting
101	2110344.6682	6822673.3743
109	2110489.4287	6822608.0718
103	2110392.3105	6822636.9665
104	2110338.5214	6822621.7453
105	2110421.4818	6822633.8296
106	2110440.8709	6822620.3364
107	2110460.2734	6822606.9653
108	2110438.2210	6822533.0647

Point Table		
Point #	Northing	Easting
110	2110496.7672	6822642.8173
111	2110479.8715	6822650.0502
112	2110462.5312	6822651.8648
113	2110474.9982	6822605.2991
114	2110465.9256	6822728.0396
115	2110551.7054	6822617.9778
116	2110443.3075	6822642.7111
117	2110449.0927	6822630.7510

Point Table		
Point #	Northing	Easting
118	2110461.6788	6822619.3860
119	2110463.8322	6822639.1604
120	2110477.7047	6822617.6522
121	2110478.6371	6822637.6111
122	2110498.2048	6822625.5591
123	2110488.4716	6822633.4648
124	2110483.5531	6822619.1048
125	2110365.3155	6822639.9062



CITY OF AUBURN



Jacobs



ISSUE RECORD			
NO.	BY	DATE	DESCRIPTION

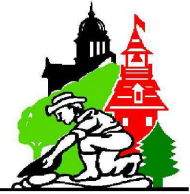
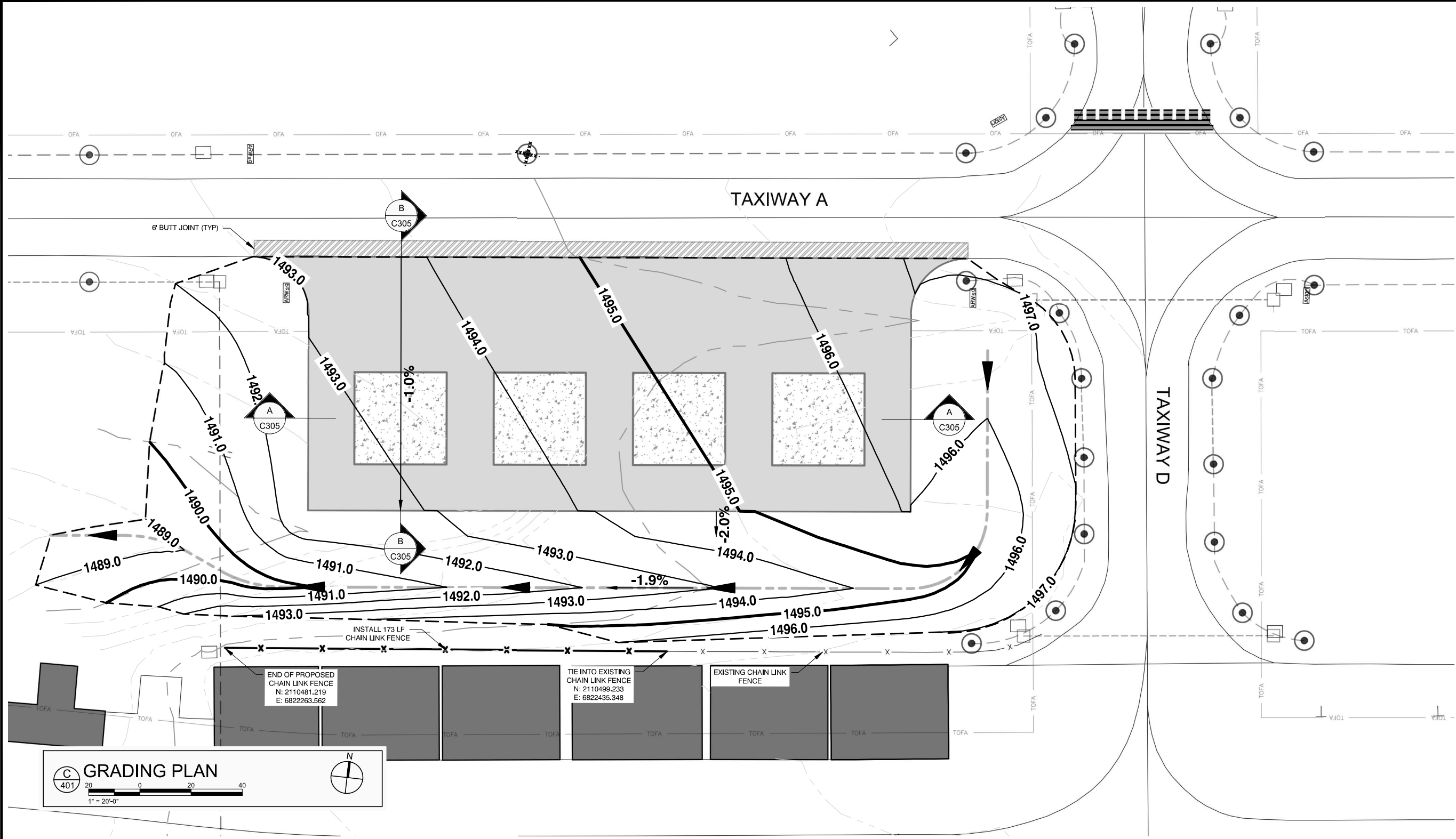
AIP PROJECT NUMBER: 3-06-0012-017	
JACOBS PROJECT NUMBER: W9Y24804	DATE: 11/25/19
DESIGNED: AW	DRAWN: AW
CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN  
**AUBURN MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER PARKING AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE: <b>GEOMETRIC PLAN - TWY D</b>	
DATE 24 FEBRUARY 2022	DRAWING NO. <b>C302</b>
SHEET NUMBER 10 OF 21	





CITY OF AUBURN



James Mills

Jacobs



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ISSUE RECORD

NO.	BY	DATE	DESCRIPTION

AIP PROJECT NUMBER:  
3-06-0012-017

JACOBS PROJECT NUMBER:  
W9Y24604

DATE:  
11/25/19

DESIGNED: AW  
DRAWN: AW  
CHECKED: JLW  
APPROVED: JJM

CITY OF AUBURN

AUBURN  
MUNICIPAL AIRPORT  
Auburn, California, 95603  
(530) 823-4211

PLANNING AND PUBLIC WORKS

PROJECT TITLE:

HELICOPTER  
PARKING  
AREA

AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:

GRADING  
PLAN

DATE  
24 FEBRUARY 2022

DRAWING NO.

C401

SHEET NUMBER  
11 OF 21

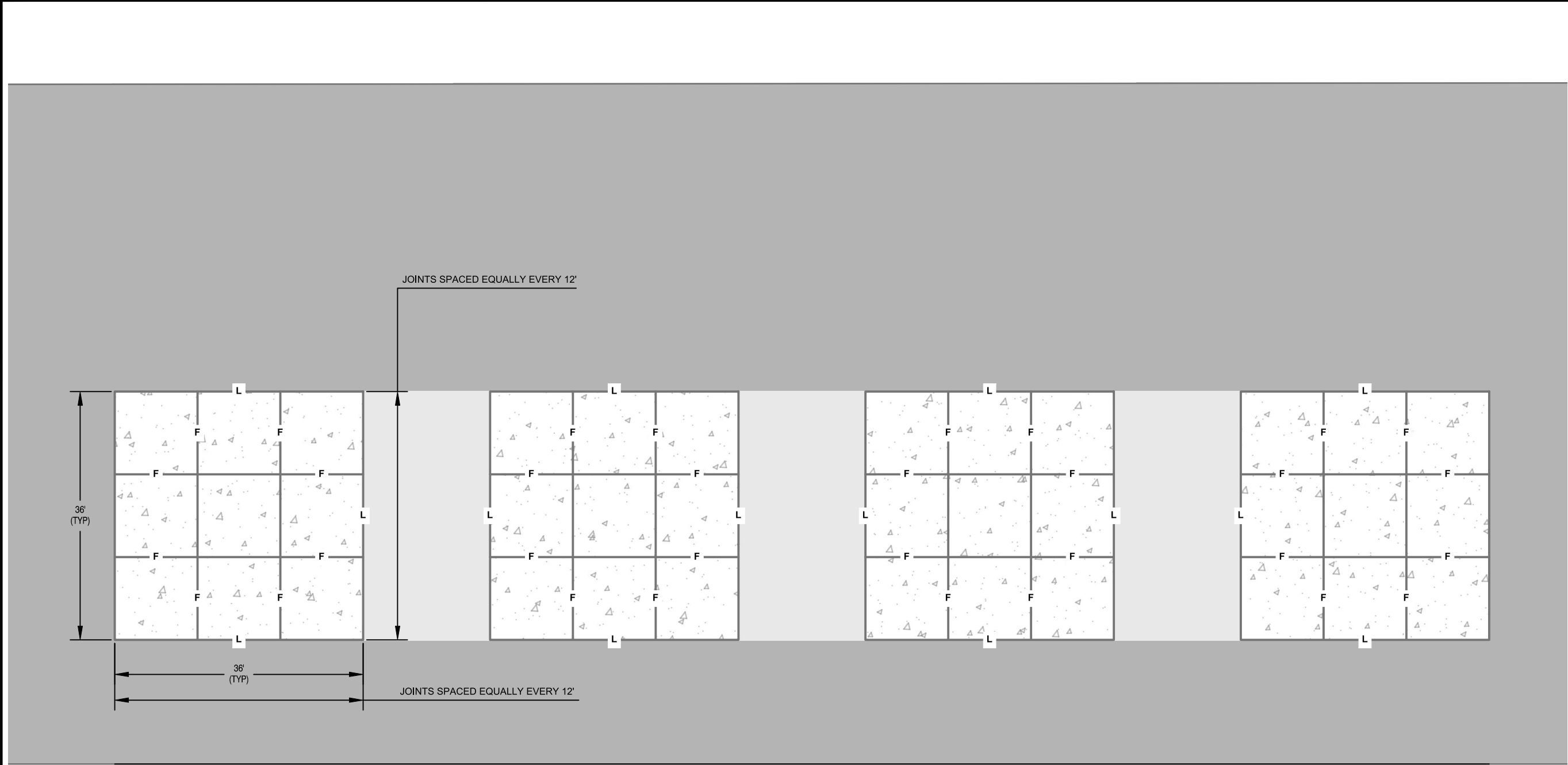
LEGEND

- EXISTING BUILDING
- EXISTING CHAIN LINK FENCE
- PROPOSED CHAIN LINK FENCE
- PROPOSED ASPHALT SURFACE
- PROPOSED CONCRETE PAD
- PROPOSED BUTT JOINT

- PROPOSED FLOW LINE
- PROPOSED GRADING EXTENTS/BOUNDARY
- PROPOSED GRADING - MAJOR CONTOUR
- PROPOSED GRADING - MINOR CONTOUR

NOTES

- ALL GRADING SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF AUBURN BUILDING CODE.
- THE CONTRACTOR SHALL OBTAIN ANY NECESSARY CITY AND BASE PERMITS PRIOR TO STARTING WORK.
- NO GRADING WORK SHALL BE PERFORMED AT ANY TIME WITHOUT AN EROSION CONTROL SYSTEM APPROVED BY THE CITY OF AUBURN.
- THE CONTRACTOR SHALL PROVIDE EROSION AND SEDIMENTATION CONTROL. USE RECOMMENDATIONS BY THE BEST MANAGEMENT PRACTICES (BMPs) TO IMPLEMENT CONTROL.
- WHERE NO SLOPE GRADIENT IS SHOWN, PROVIDE STRAIGHT GRADE BETWEEN CONTOURS.
- THE CONTRACTOR SHALL COMPACT THE TOP 12" OF THE SUB-GRADE TO 90% MAXIMUM RELATIVE DENSITY.



C1 PAVEMENT JOINT PLAN  
NTS

LEGEND

	PROPOSED ASPHALT SURFACE
	PROPOSED CONCRETE JOINT
	PROPOSED CONCRETE PAD

NOTES

- ALL CONCRETE JOINTS ARE SPACED EQUALLY EVERY 12 FEET.
- SEE SHEET C502 FOR ALL JOINT DETAILS.



Jacobs



ISSUE RECORD			
NO.	BY	DATE	DESCRIPTION

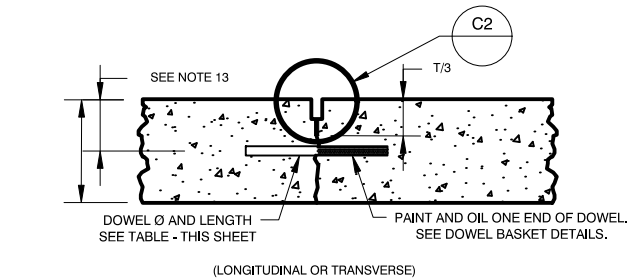
AIP PROJECT NUMBER: 3-06-0012-021-2022	
JACOBS PROJECT NUMBER: W9Y24604	DATE: 11/25/19
DESIGNED: AW	DRAWN: AW
CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN  
**AUBURN  
MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

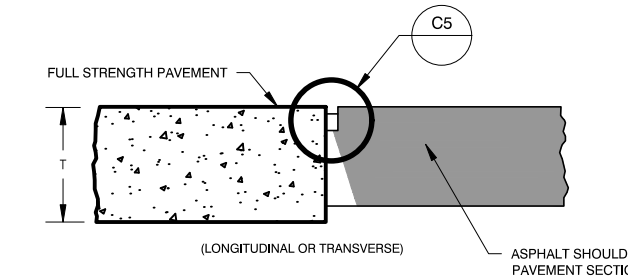
PROJECT TITLE:  
**HELICOPTER  
PARKING  
AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
**PAVEMENT JOINT  
LAYOUT PLAN**

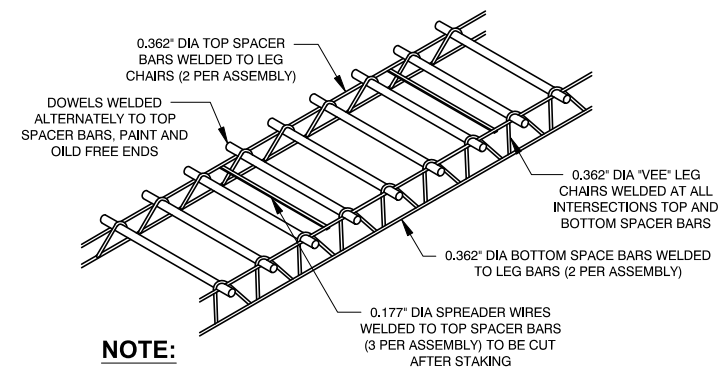
DATE 11 FEBRUARY 2022	DRAWING NO. <b>C501</b>
SHEET NUMBER 11 OF 21	



C1 TYPE "F" DOWELED CONTRACTION JOINT  
NTS



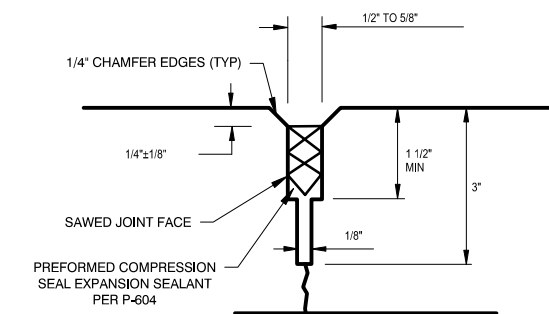
C3 TYPE "L" PCC TO ACC CONSTRUCTION JOINT  
NTS



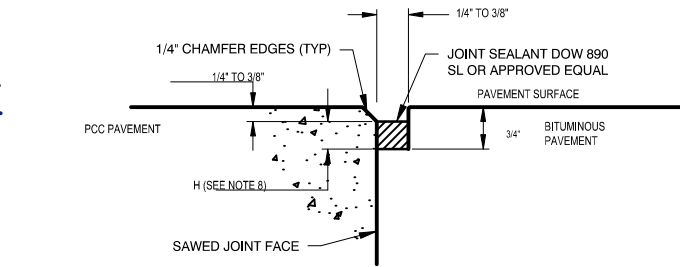
**NOTE:**

- BASKET SHALL BE FIRMLY ATTACHED TO GRADE.
- ALL WIRE SIZES SHOWN ARE MINIMUM SIZE.
- DOWELS SHALL BE HELD FIRMLY IN THE ASSEMBLY.

C4 DOWEL BASKET ASSEMBLY  
NTS



C2 CONTRACTION JOINT  
NTS



C5 CONCRETE/BITUMINOUS JOINT DETAIL  
NTS

**NOTE:**

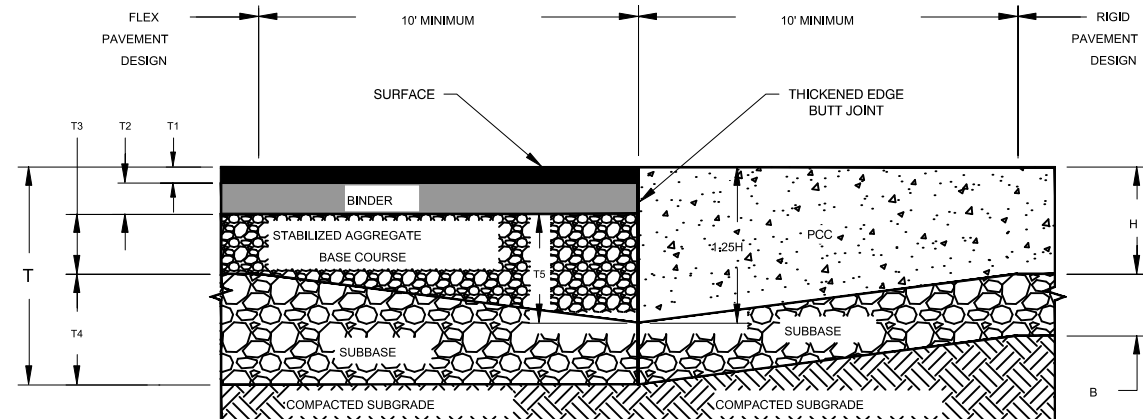
- THE JOINT SEALANT FOR THIS APPLICATION SHALL BE DOW 890 SL OR APPROVED EQUAL. THE JOINT SEALANT AND APPLICATION SHALL MEET ALL MANUFACTURE'S REQUIREMENTS. THE JOINT SHALL BE WIDENED CLEANED AND PREPARED IN THE SAME MANNER AS ALL CONCRETE JOINTS.
- CONTRACTOR SHALL ENSURE THAT ROLLING EQUIPMENT IS KEPT OFF OF THE CONCRETE EDGE. HOWEVER, USE OF A RUBBER TIRE ROLLER IS ACCEPTABLE ALONG THE CONCRETE EDGE. ADDITIONALLY, IN ORDER TO ENSURE POSITIVE DRAINAGE OFF OF THE PCC EDGE, THE BITUMINOUS SIDE OF THE INTERFACE SHALL BE ROLLED UNTIL EITHER FLUSH IN GRADE WITH THE CONCRETE EDGE, OR SLIGHTLY BELOW THE CONCRETE EDGE.

#### JOINT DETAIL NOTES:

- JOINTS SHALL NOT INTERSECT THE EDGE OF THE PAVEMENT NOR ANY OTHER JOINT AT AN ANGLE OF LESS THAN 90°.
- DOWEL BASKETS SHALL BE FIRMLY ATTACHED TO THE EXISTING UNDERLYING COURSE PRIOR TO PLACING PCCP.
- IF THE CONTRACTOR ELECTS TO VARY FROM THE PLAN, THEY SHALL SUBMIT FOR THE C.O.'S APPROVAL A JOINT TYPE PATTERN CONSISTENT WITH THE NEW PLAN.
- PAVEMENT AREAS WHERE THICKENED EDGE JOINTS OCCUR SHALL BE CONSTRUCTED SO THAT DOWEL BARS ARE PLACED PARALLEL TO THE FINAL SURFACE AND MAINTAIN A MINIMUM DEPTH OF  $T/2 + 1/2"$  FROM THE FINAL SURFACE.
- ANY PAVEMENT SHOWN WITH THICKENED EDGE JOINTS WILL BE PAID FOR AS NOMINAL THICKNESS (T) PAVEMENT.
- ALL PREFORMED COMPRESSION SEAL JOINTS SHALL BE WIDENED AND SEAL PLACED  $1/8"$  BELOW FINISHED SURFACE. COMPRESSION SEALS MUST BE IN COMPRESSION AT ALL TIMES.
- DOWEL DRILLING METHOD SHALL BE CAPABLE OF MAINTAINING DRILL HOLES PARALLEL TO THE CONCRETE SURFACE AND NORMAL TO THE JOINT LINE WITHIN  $1/8" \pm$  AT THE END OF THE DOWEL EXCEPT WHERE SPECIFIED OTHERWISE. DRILL HOLES SHALL BE ACCURATELY LAID OUT SO THAT THE MAXIMUM DEVIATION DOES NOT EXCEED  $1"$  IN THE HORIZONTAL DIRECTION AND  $1/2"$  IN THE VERTICAL DIRECTION. DRILL HOLE DIAMETER TO BE APPROXIMATELY  $1-7/8"$  FOR  $1-1/2"$  DOWELS. BAR POSITIONING SHALL BE WITHIN A  $1/8"$  TOLERANCE IN HORIZONTAL AND VERTICAL PLANES.
- SEAL DIMENSION H IS BASED ON MANUFACTURER RECOMMENDATIONS.
- FOR PREFORMED COMPRESSED SEAL - LUBRICATION TO BE APPLIED TO BOTH SIDES OF JOINT PER MANUFACTURER'S RECOMMENDATIONS. SEAL SHALL BE COMPATIBLE WITH EXPANSION JOINT FILLER MATERIAL.
- CONCRETE FACES FOR B JOINTS SHALL BE CONSTRUCTED VERTICALLY.
- THE DOWEL BAR OR TIE BAR SPACING SHALL BE STARTED FROM THE INSIDE OF THE JOINT EDGE. IF THE SPACING DOES NOT ALLOW FOR A DOWEL AT THE CENTER OF THE SLAB AND THE REMAINING SLAB IS GREATER THAN THE REQUIRED SPACING THEN A BAR SHALL BE PLACED IN THE CENTER OF THE REMAINING SPACE. IF THE REMAINING SPACE IS LESS THAN THE REQUIRED SPACING, NO ADDITIONAL BAR SHALL BE REQUIRED.
- PLACEMENT OF NEW DOWELS OR TIES SHALL BE PLACED ADJACENT TO EXISTING DOWELS OR TIES SAWCUT DURING REMOVAL OPERATIONS BUT NO CLOSER THAN 2 BAR DIAMETERS.
- PLACE DOWELS FOR PCCP ADJACENT TO EXISTING PANELS 6.5" FROM TOP OF PCCP SLAB.

DIMENSIONS AND SPACING OF STEEL DOWELS			
SLAB THICKNESS	DIAMETER	LENGTH	SPACING
12"-14"	1 1/4"	20"	15"

**NOTE:**  
DOWEL BAR SPACING SHALL BE SUCH THAT BARS BEGIN AND END ONE SPACING LENGTH FROM TRANSVERSE JOINTS.



C6 PAVEMENT SECTION TRANSFER DETAIL  
NTS

#### DIMENSION DESCRIPTION

H	DESIGN THICKNESS OF CONCRETE PAVEMENT	T2	DESIGN THICKNESS OF BINDER COURSE
B	THICKNESS OF BASE	T3	DESIGN THICKNESS OF BASE COURSE
T	DESIGN THICKNESS OF FLEXIBLE ASPHALT PAVEMENT	T4	DESIGN THICKNESS OF SUBBASE COURSE
T1	DESIGN THICKNESS OF SURFACE COURSE	T5	(H + B) - (T1 + T2) OR (T3), WHICHEVER IS GREATER



Jacobs



#### ISSUE RECORD

NO.	BY	DATE	DESCRIPTION

AIP PROJECT NUMBER: 3-06-0012-021-2022	
JACOBS PROJECT NUMBER: W9Y24604	DATE: 11/25/19
DESIGNED: AW	DRAWN: AW
CHECKED: JLW	APPROVED: JIM

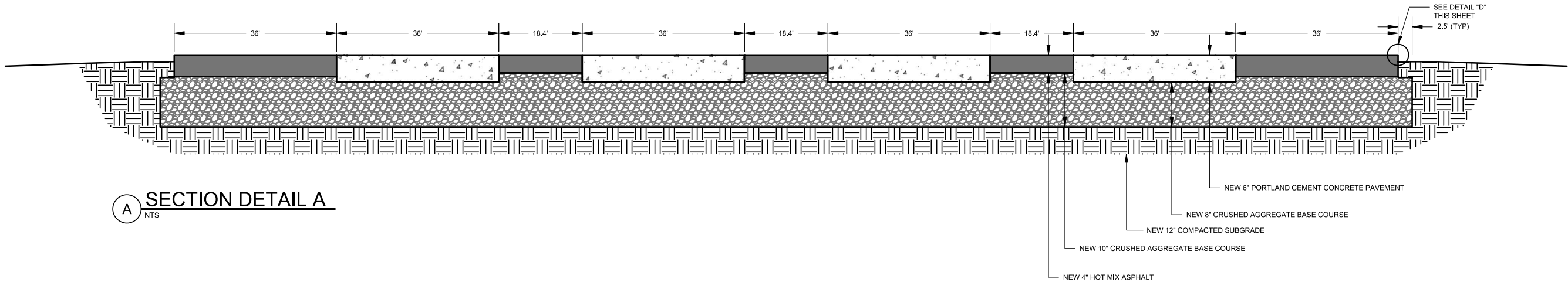
CITY OF AUBURN  
**AUBURN MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER PARKING AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

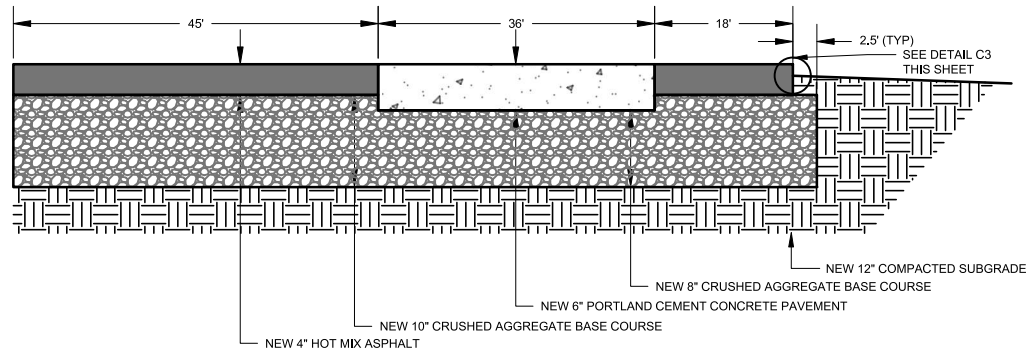
DRAWING TITLE:  
**PAVEMENT JOINT DETAILS**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>C502</b>
SHEET NUMBER 12 OF 21	

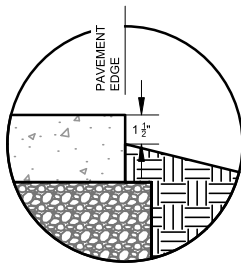




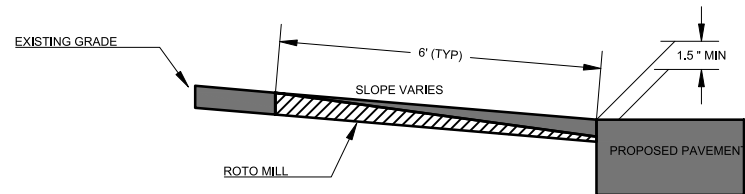
**A** SECTION DETAIL A  
NTS



**B** SECTION DETAIL B  
NTS



**C3** EDGE DETAIL  
NTS



**E** BUTT JOINT DETAIL



**Jacobs**



Know what's below.  
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ISSUE RECORD			
NO.	BY	DATE	DESCRIPTION

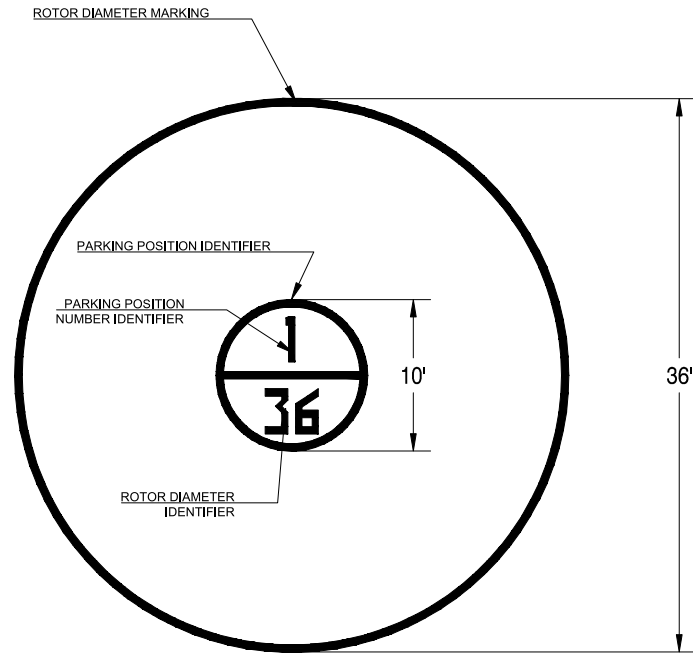
AIP PROJECT NUMBER: 3-06-0012-021-2022	
JACOBS PROJECT NUMBER: W9Y24604	DATE: 11/25/19
DESIGNED: AW	DRAWN: AW
CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN  
**AUBURN MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

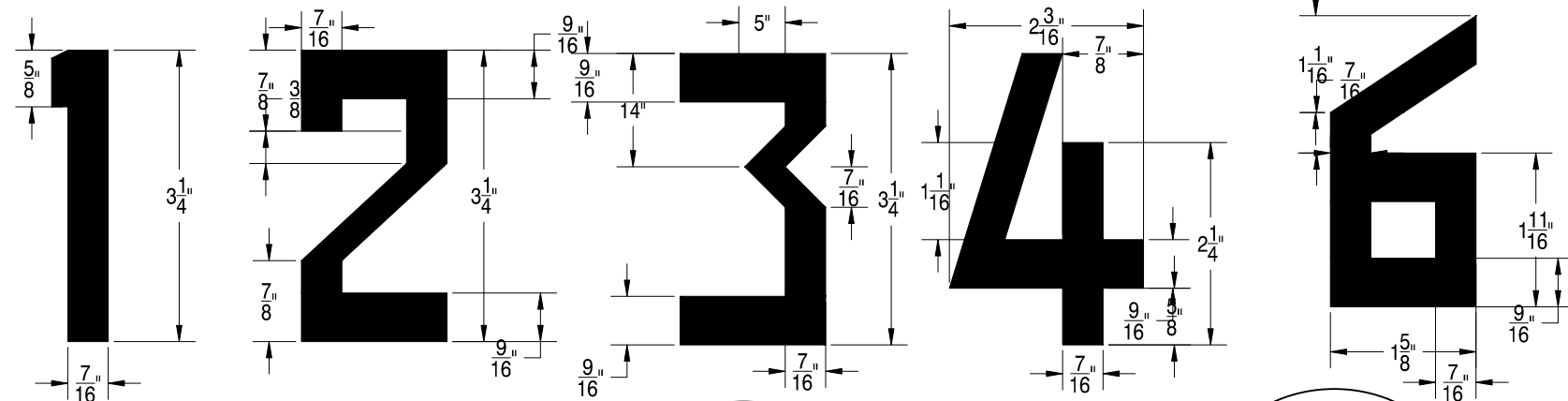
PROJECT TITLE:  
**HELICOPTER PARKING AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
**PAVEMENT SECTIONS**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>C305</b>
SHEET NUMBER 13 OF 21	



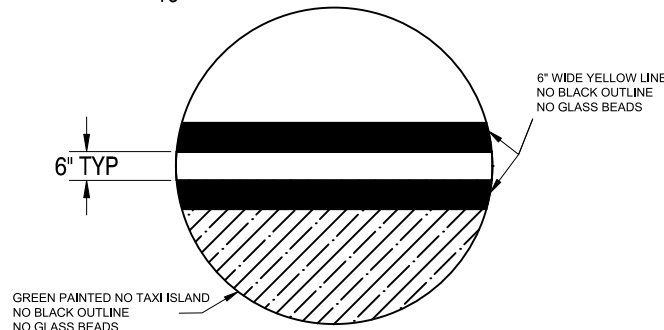
C1 PAINT MARKING DETAIL  
NTS



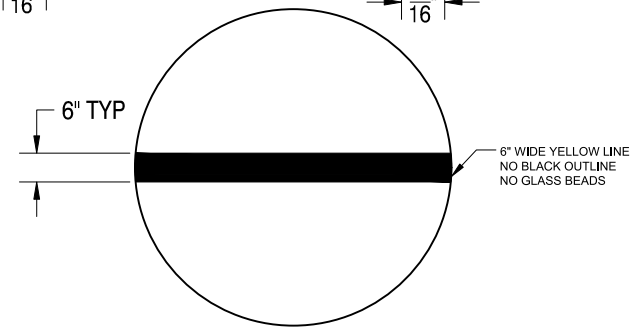
NOTES:  
All characters have the following characteristics (Unless otherwise specified):

36 in [91cm] high      Vertical stroke of 5 in [13 cm]  
18 in [46 cm] wide      Horizontal stroke of 6 in [15 cm]

C2 NUMBER MARKING DETAIL  
NTS

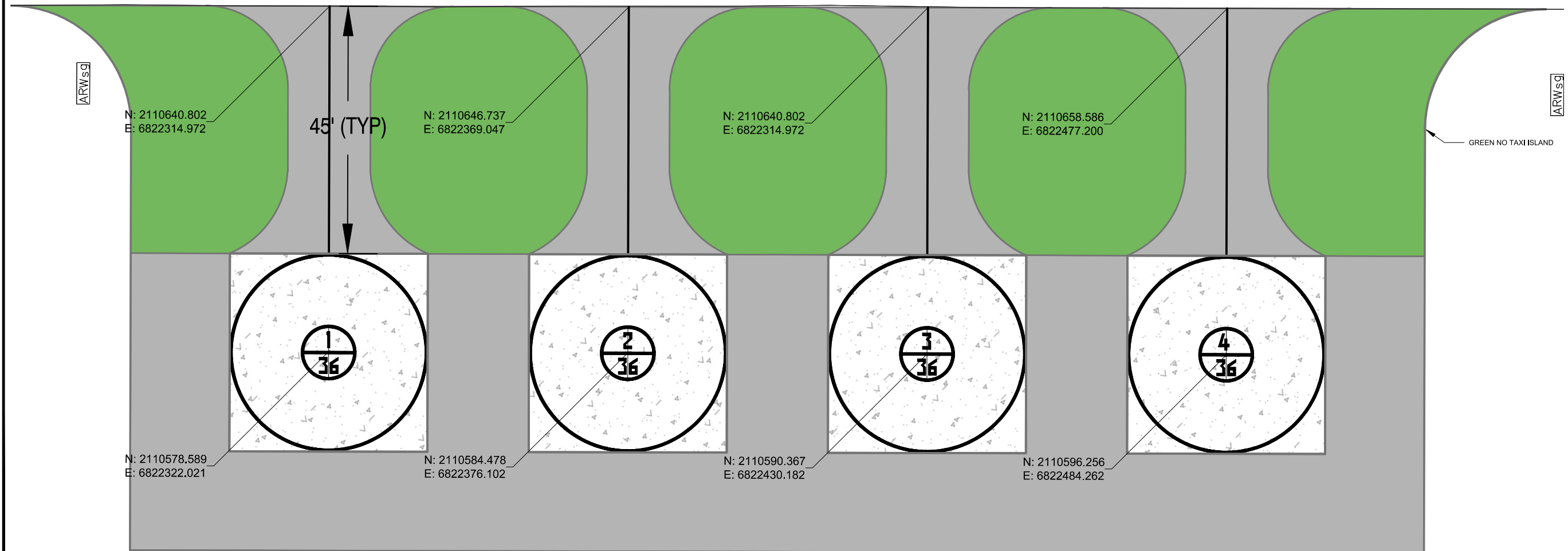


C3 "NO TAXI" ISLANDS WITH BORDER DETAIL  
NTS



C4 6" WIDE YELLOW LINE MARKING DETAIL  
NTS

NOTES:  
1. MARKINGS ARE PAINTED YELLOW.  
2. COORDINATE WITH AIRPORT MANAGER TO VERIFY NUMBER ORDER.  
3. CENTER ROTOR DIAMETER MARKING AND PARKING POSITION IDENTIFIER ON CONCRETE PAD.  
4. PARKING POSITION IDENTIFIER NUMBER AND ROTOR DIAMETER IDENTIFIER NUMBER TO BE PLACED IN THE CENTER OF THEIR RESPECTIVE AREAS.



Jacobs



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Call before you dig.

ISSUE RECORD

NO.	BY	DATE	DESCRIPTION

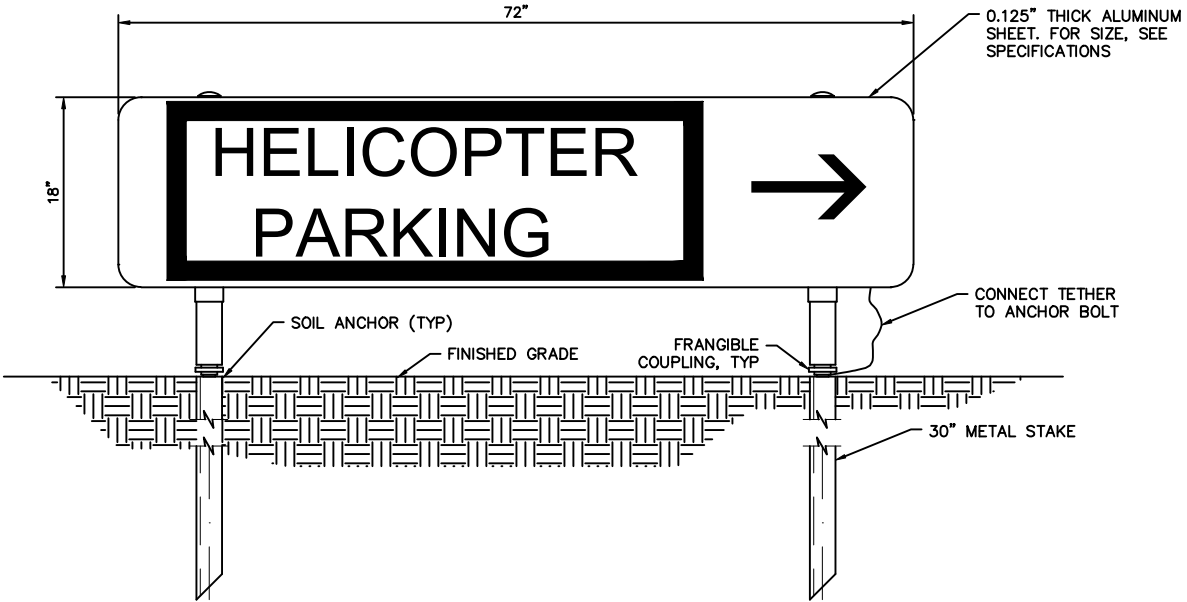
AIP PROJECT NUMBER: 3-06-0012-017			
JACOBS PROJECT NUMBER: W9Y24604		DATE: 11/25/19	
DESIGNED: AW	DRAWN: AW	CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN  
**AUBURN  
MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER  
PARKING  
AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
**PAINT MARKINGS  
PLAN**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>C504</b>
SHEET NUMBER 14 OF 21	



- NOTE:
1. PROVIDE SIGN AND FRANGIBLE COUPLINGS RATED FOR MODE 2 (200 M.P.H.).

**C1** **STYLE 4 - UNLIGHTED SIGN**  
NTS

- NOTES:
1. UNLIGHTED GUIDANCE SIGNS SHALL HAVE YELLOW BACKGROUND WITH BLACK LETTERING.
  2. SIGN LETTERING SIZE SHALL BE IN ACCORDANCE WITH AC 150/5340-18G AND AC 150/5345-44K. LETTERING SIZE SHALL BE SCALED TO FIT THE SIGN AS SHOWN IN THE DETAILS.



CITY OF AUBURN



**Jacobs**



ISSUE RECORD			
NO.	BY	DATE	DESCRIPTION

AIP PROJECT NUMBER: 3-06-0012-017			
JACOBS PROJECT NUMBER: W9Y24604		DATE: 11/25/19	
DESIGNED: AW	DRAWN: AW	CHECKED: JLW	APPROVED: JJM

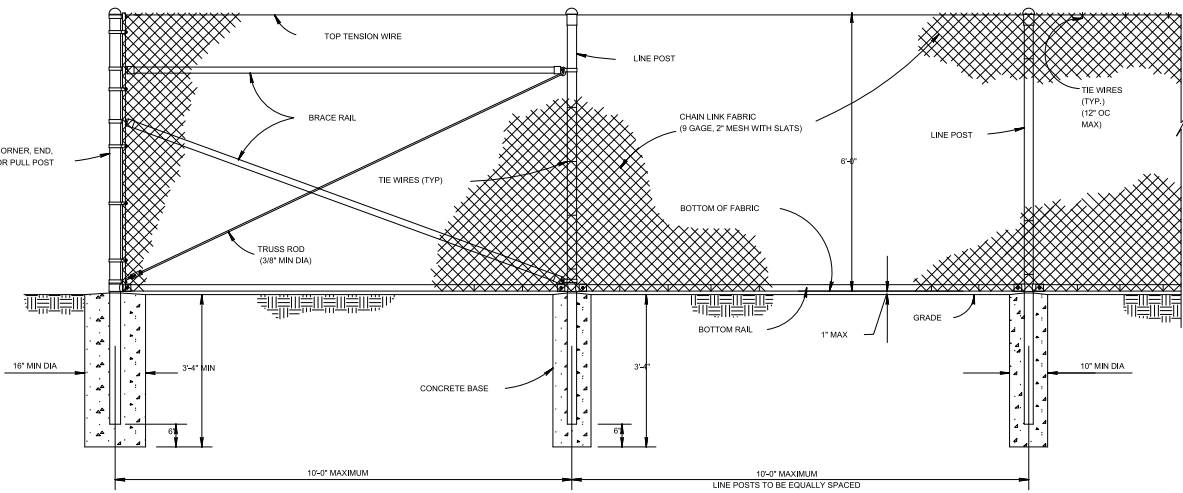
CITY OF AUBURN  
**AUBURN  
MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER  
PARKING  
AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

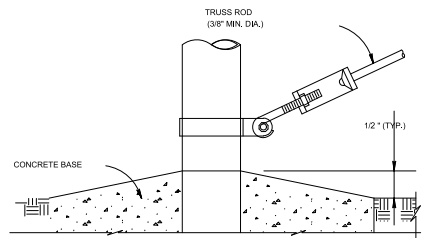
DRAWING TITLE:  
**GAUIDANCE SIGN  
DETAILS**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>C505</b>
SHEET NUMBER 15 OF 21	

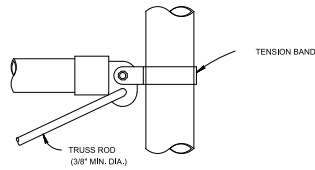




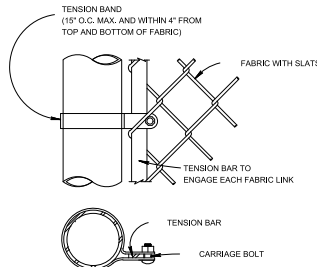
C1 CHAIN LINK SECURITY FENCE WITH SLATS DETAIL



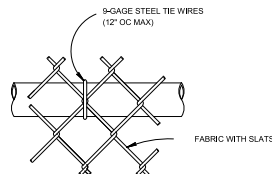
C3 TRUSS ROD AND BAND



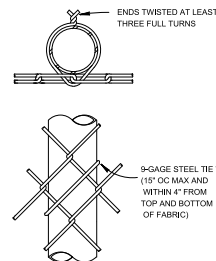
C4 BRACE RAIL CLAMP DETAIL



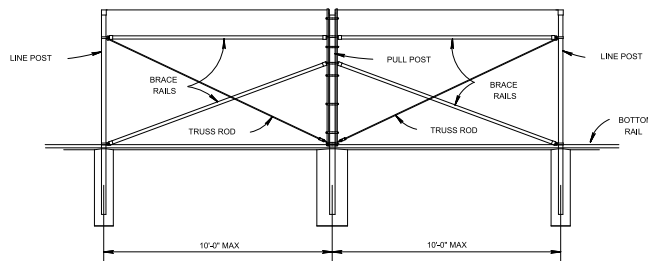
C5 END OR POST DETAIL



C6 BRACE RAIL ATTACHMENT

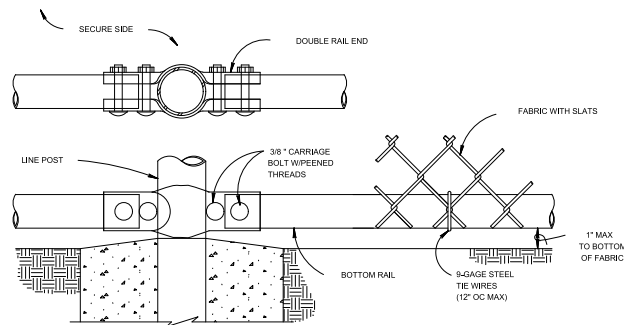


C7 LINE POST ATTACHMENTS



C8 BRACE PANEL DETAIL

NOTE:  
PROVIDE BRACE PANEL WHENEVER  
STRAIGHT RUNS EXCEED 500 FEET.



C9 BOTTOM RAIL DETAILS

STEEL POST SCHEDULE			
USE AND SECTION	MINIMUM OUTSIDE DIMENSIONS (NOMINAL)		
	FABRIC WIDTH 72" OR LESS	FABRIC WIDTH 84" TO 96"	FABRIC WIDTH 108" AND OVER
CORNER, END & PULL POSTS			
TUBULAR - ROUND	2.375" OD	2.875" OD	4.00" OD
LINE POSTS			
TUBULAR - ROUND	1.90" OD	2.375" OD	2.875" OD
BOTTOM & BRACE RAILS			
TUBULAR - ROUND		1.66" OD	
TUBULAR - SQUARE		1.50" SQ	
H-SECTION		1.625" x 1.50"	
C-SECTION (ROLL-FORMED)		1.625" x 1.25"	

NOTES:

1. WIRE TIES, RAILS, POSTS, AND BRACES SHALL BE CONSTRUCTED ON THE SECURE SIDE OF THE FENCE ALIGNMENT. CHAIN LINK FABRIC SHALL BE PLACED ON THE SIDE OPPOSITE THE SECURE AREA.

2. ONLY 9-GAGE GALVANIZED STEEL TIE WIRES SHALL BE USED FOR FASTENING THE FENCE FABRIC TO FENCE POSTS AND RAILS. 16-GAGE, STAINLESS STEEL TIE WIRES SHALL BE USED FOR FASTENING FENCE FABRIC TO TENSION WIRES. HOG RINGS SHALL NOT BE ALLOWED ON SENSORED FENCES.

3. BOTTOM RAIL SHALL BE ATTACHED TO DOUBLE RAIL ENDS USING 3/8\"/>

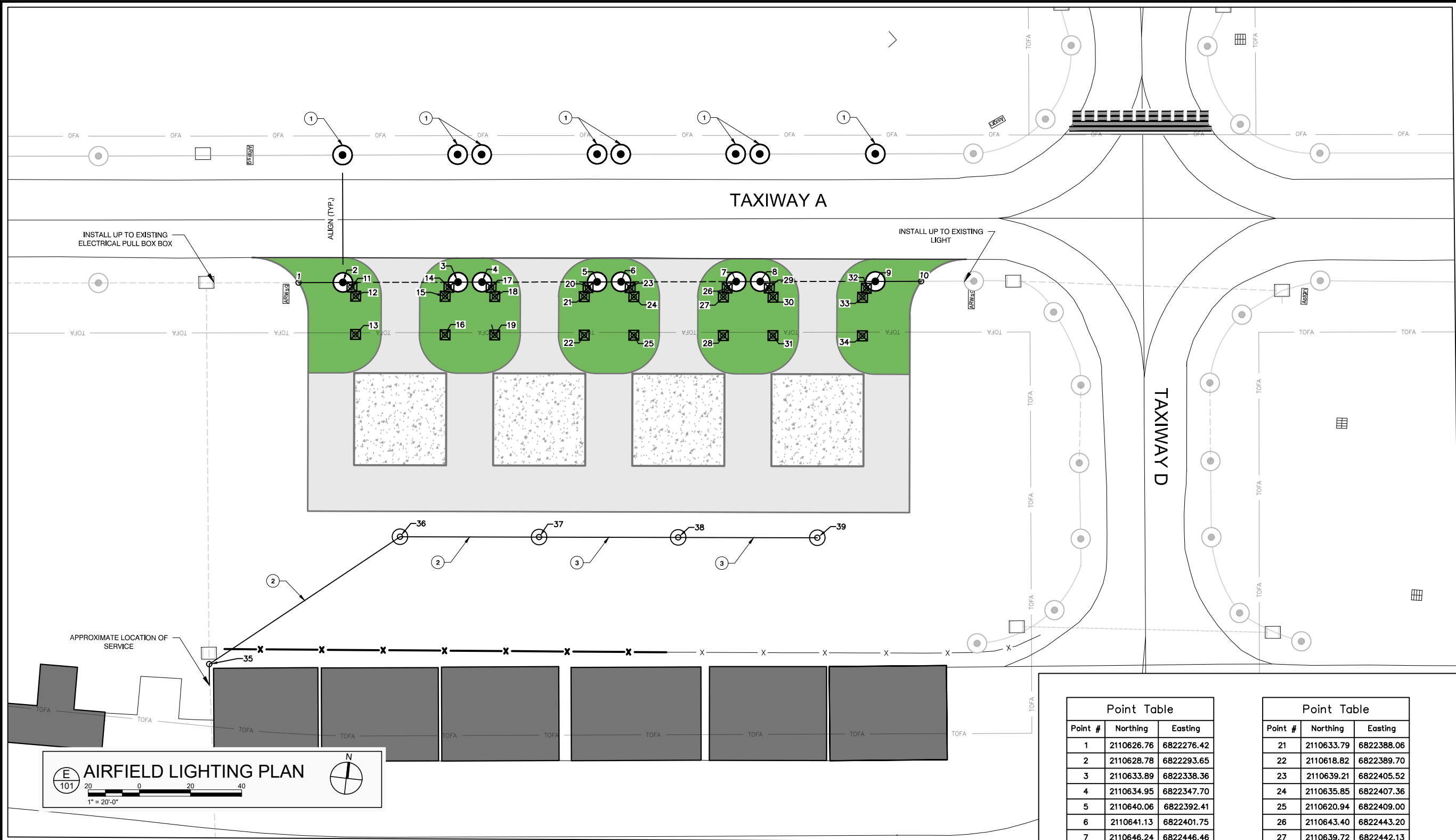
FENCE LEGEND:

TYPE FES - CHAIN LINK FENCE WITHOUT BARBED-WIRE APRON  
TYPE FE6 - CHAIN LINK FENCE W/BARBED-WIRE ON SINGLE OUTRIGGER  
TYPE FE7 - CHAIN LINK FENCE W/BARBED-WIRE ON DOUBLE OUTRIGGER  
TYPE FE8 - CHAIN LINK FENCE W/BARBED-WIRE AND BARBED-TAPE ON DOUBLE OUTRIGGER  
TR - FENCE WITH TOP RAIL AND TENSION WIRE AT BOTTOM  
TBR - FENCE WITH TOP AND BOTTOM RAILS  
TWB - FENCE WITH TOP TENSION WIRE AND BOTTOM RAIL  
FINAL NUMBER IS FABRIC WIDTH IN INCHES.

EXAMPLES:

FE6-TR-72 - CHAIN LINK SECURITY FENCE WITH BARBED-WIRE ON SINGLE OUTRIGGER, TOP RAIL, AND 72 INCH FABRIC WIDTH.

FES-TWB-84 - CHAIN LINK SECURITY FENCE WITH NO APRON, TOP AND BOTTOM TENSION WIRE, AND 84 INCH FABRIC WIDTH.



LEGEND

- INSTALL TAXIWAY EDGE REFLECTOR
- INSTALL 1-WAY, 2-INCH PVC, CONCRETE ENCASED DUCT. SEE SHEET E102, DETAIL 3.
- INSTALL 1-WAY, 2-INCH PVC, DIRECT EARTH BURIED DUCT. UNLESS NOTED OTHERWISE.
- INSTALL L-852T(L), LED, TAXIWAY EDGE LIGHT ON NEW L-868, SIZE B BASE CAN, UNLESS NOTED OTHERWISE. SEE SHEET E102, DETAIL 4.
- DUPLEX CONVENIENCE RECEPTACLE WITH WEATHERPROOF COVER, MOUNTED TO STRUT - 4' AFG

- PROPOSED ASPHALT SURFACE
- PROPOSED CONCRETE PAD
- PROPOSED GREEN PAINTED "NO TAXI" ISLAND

NOTES

- 1 REMOVE/RE-INSTALL L-861T(L), LED, TAXIWAY EDGE LIGHT ON NEW L-867, SIZE B BASE CAN.
- 2 INSTALL 2-WAY, 2-INCH PVC (DEB) WITH 2-1/C #4 XHHW, 1#4 GND AND 2-1/C #2 XHHW, 1#2 GND.
- 3 INSTALL 1-WAY, 2-INCH PVC (DEB) WITH 2-1/C #2 XHHW, 1#2 GND.
4. FOR ELECTRICAL DEMOLITION, SEE SHEET C201.
5. FOR PAVEMENT MARKING PLAN, SEE SHEET C504.

Point Table		
Point #	Northing	Easting
1	2110626.76	6822276.42
2	2110628.78	6822293.65
3	2110633.89	6822338.36
4	2110634.95	6822347.70
5	2110640.06	6822392.41
6	2110641.13	6822401.75
7	2110646.24	6822446.46
8	2110647.30	6822455.80
9	2110652.41	6822500.51
10	2110654.45	6822518.32
11	2110627.44	6822297.16
12	2110623.98	6822299.21
13	2110609.13	6822300.83
14	2110631.61	6822334.96
15	2110627.80	6822334.00
16	2110612.89	6822335.64
17	2110633.33	6822351.43
18	2110629.92	6822353.29
19	2110615.01	6822354.92
20	2110637.41	6822389.15

Point Table		
Point #	Northing	Easting
21	2110633.79	6822388.06
22	2110618.82	6822389.70
23	2110639.21	6822405.52
24	2110635.85	6822407.36
25	2110620.94	6822409.00
26	2110643.40	6822443.20
27	2110639.72	6822442.13
28	2110624.76	6822443.79
29	2110645.20	6822459.58
30	2110641.79	6822461.44
31	2110626.88	6822463.08
32	2110649.34	6822497.28
33	2110645.66	6822496.21
34	2110630.69	6822497.87
35	2110474.66	6822258.38
36	2110532.46	6822326.93
37	2110538.39	6822381.01
38	2110544.33	6822435.08
39	2110550.26	6822489.16



CITY OF AUBURN



February 24, 2022

Jacobs



Know what's below.  
Call before you dig.

ISSUE RECORD

NO.	BY	DATE	DESCRIPTION

AIP PROJECT NUMBER: 3-06-0012-017			
JACOBS PROJECT NUMBER: WSY24604		DATE: 11/25/19	
DESIGNED: AW	DRAWN: AW	CHECKED: JLW	APPROVED: JJM

CITY OF AUBURN

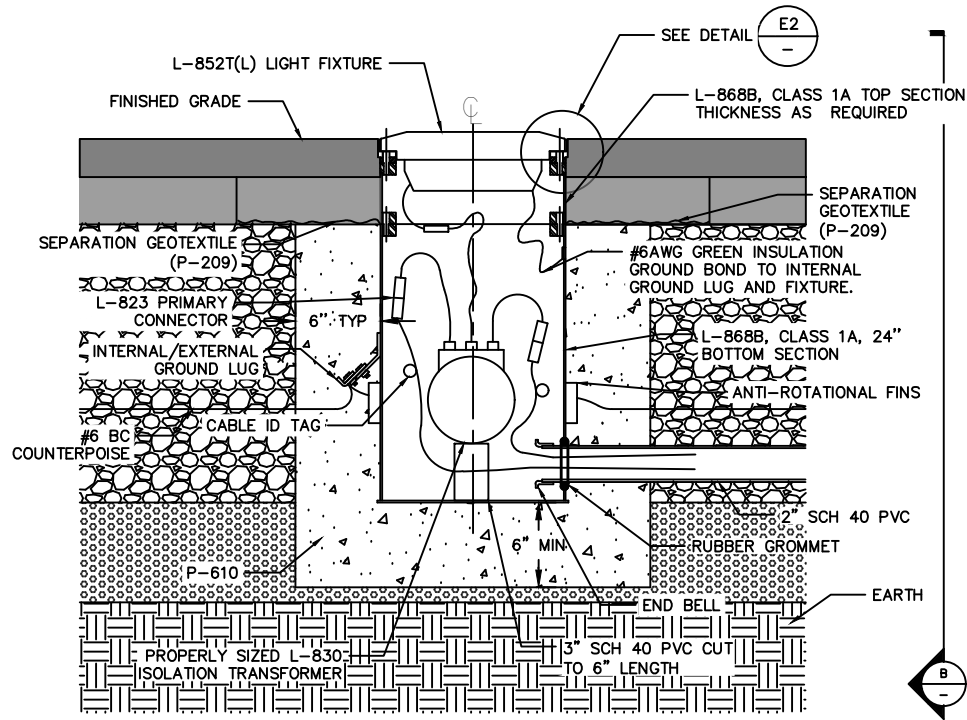
AUBURN  
MUNICIPAL AIRPORT  
Auburn, California, 95603  
(530) 823-4211

PLANNING AND PUBLIC WORKS

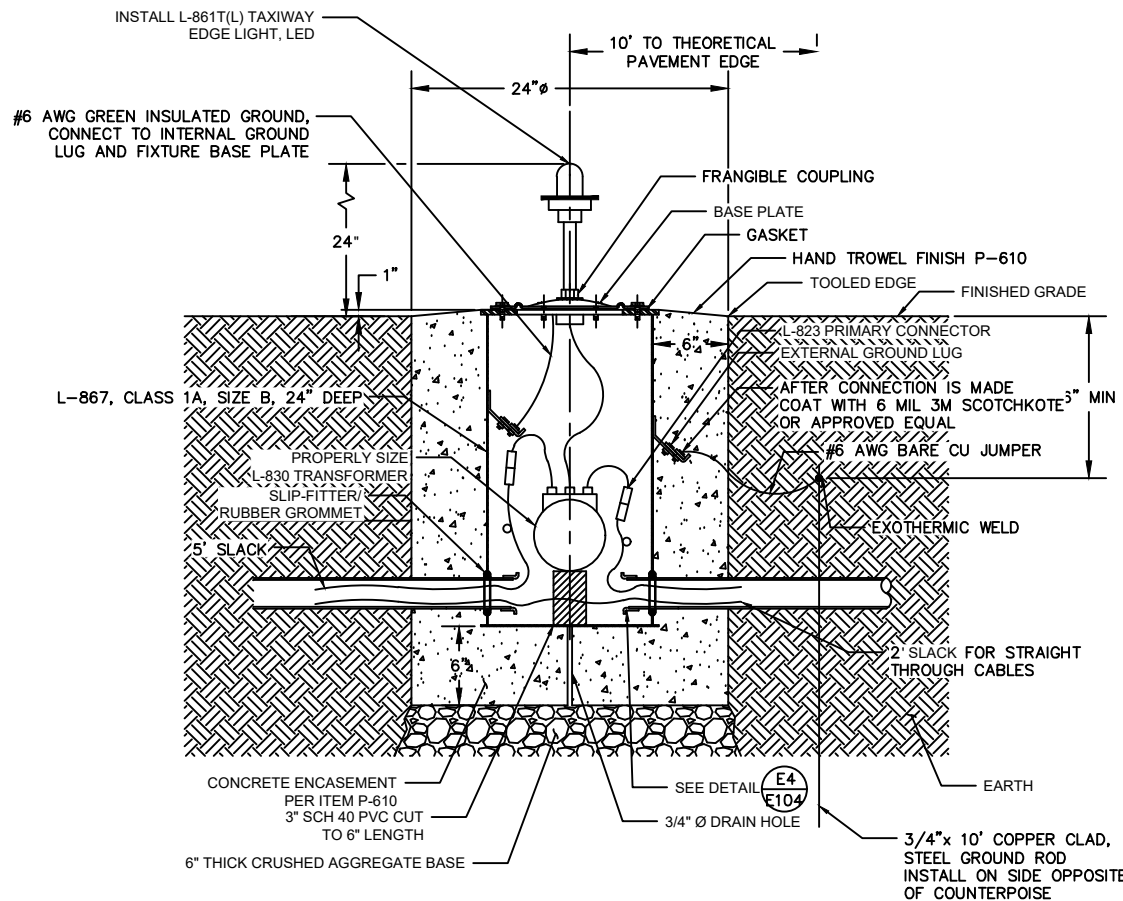
PROJECT TITLE:  
**HELICOPTER  
PARKING  
AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
**AIRFIELD  
LIGHTING  
PLAN**

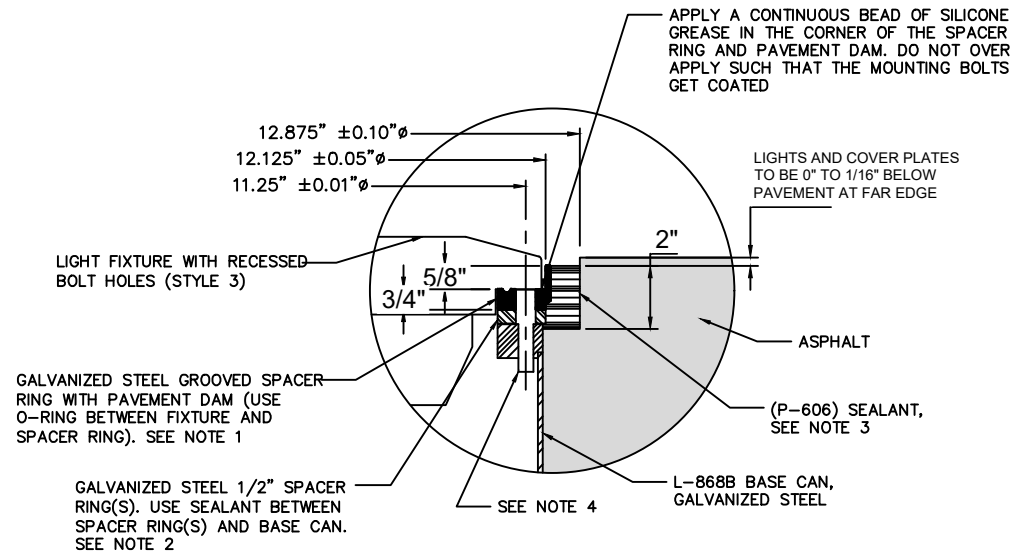
DATE 24 FEBRUARY 2022	DRAWING NO. <b>E101</b>
SHEET NUMBER 18 OF 21	



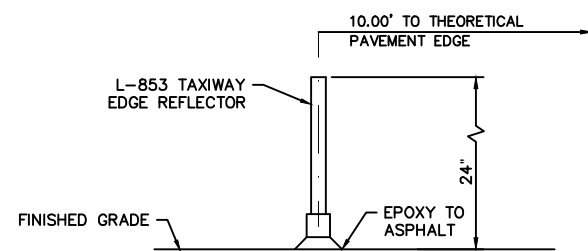
**E1** INSTALLATION DETAIL FOR INSET RUNWAY GUARD LIGHT  
NTS



**E3** ELEVATED TAXIWAY EDGE LIGHT IN TURF  
NTS



**E2** DETAIL FOR L-868B BASE CAN IN AC  
NTS



**E4** ELEVATED TAXIWAY  
EDGE REFLECTOR - SURFACE MOUNTED  
NTS

**NOTES:**

1. THE GROOVED SPACER RING IS 3/4" THICK WITH AN ATTACHED Y-FLANGE 5/8" ABOVE THE SPACE. GROOVED SPACER RING SHALL BE O-RING GASKETED WITH SURFACE OF FIXTURE.
2. FOR L-868 BASE CAN INSTALLATIONS, ADDITIONAL NOMINAL 1/2" THICK SPACER RING SHALL BE REQUIRED TO MEET THE OVERALL DEPTH OF 2" TO FINISHED GRADE. DEPENDANT ON FINAL FINISHED GRADE TOLERANCES OF  $\pm 1/2"$ , AN ADDITIONAL SPACER MAY BE REQUIRED. TO MEET OVERALL LIGHT INSTALLATION TOLERANCE OF 0" TO 1/16" BELOW GRADE AT THE LOWEST ELEVATION. A MAXIMUM OF 2 SPACER RINGS SHALL BE ALLOWED. THE SPACER RINGS MAY BE REQUIRED TO BE THINNER OR THICKER DEPENDING ON BASE CAN INSTALLATION AND PAVING TECHNIQUES. THIS CONTRACTOR SHALL BE RESPONSIBLE TO MEASURE AND DETERMINE THE EXACT REQUIRED THICKNESS OF EACH INDIVIDUAL SPACER RING REQUIRED TO PUT THE AIRFIELD LIGHTING FIXTURE AT THE CORRECT ELEVATION, AZIMUTH, AND ROTATION PER FAA ADVISORY CIRCULAR 150/5345-46 LATEST EDITION. THE CONTRACTOR'S BID PRICE SHALL INCLUDE FURNISHING AND INSTALLING NEW SPACER RINGS. COAT O-RING WITH SILICONE GREASE. SPACER RINGS ARE REQUIRED TO ALLOW FIXTURE HEIGHT ADJUSTMENTS IN THE FUTURE.
3. THE P-606 SEALANT FILL THE VOID TO BE FLUSH WITH THE ASPHALT WITHIN 0.125" BELOW THE TOP EDGE OF THE CONCRETE RING AT THE LOWEST POINT OF THE PAVEMENT. ANY OVER POURS SHALL BE REPLACED BY AND AT THE CONTRACTOR'S EXPENSE.
4. THE FIXTURE MOUNTING BOLTS SHALL EXTEND THROUGH THE BASE CAN MOUNTING FLANGE INTO THE BASE CAN A MINIMUM OF 1/2" AND MAXIMUM OF 1-1/2". THE BOLTS SHALL HAVE ENOUGH THREAD LENGTH SO THEY DO NOT SHOULDER OUT BEFORE THE FIXTURE IS SECURELY TIGHTENED. THE BOLTS SHALL BE TORQUED PER MANUFACTURER'S RECOMMENDATIONS. THE BOLTS SHALL BE SUPPLIED WITH 2 - PART LOCK WASHERS.



CITY OF AUBURN



February 24, 2022

**Jacobs**



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**ISSUE RECORD**

NO.	BY	DATE	DESCRIPTION

AIP PROJECT NUMBER: 3-06-0012-017	
JACOBS PROJECT NUMBER: W9Y24604	DATE: 11/25/19
DESIGNED: AW	DRAWN: AW
CHECKED: JLW	APPROVED: JIM

CITY OF AUBURN  
**AUBURN MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER PARKING AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
**LIGHTING DETAILS**

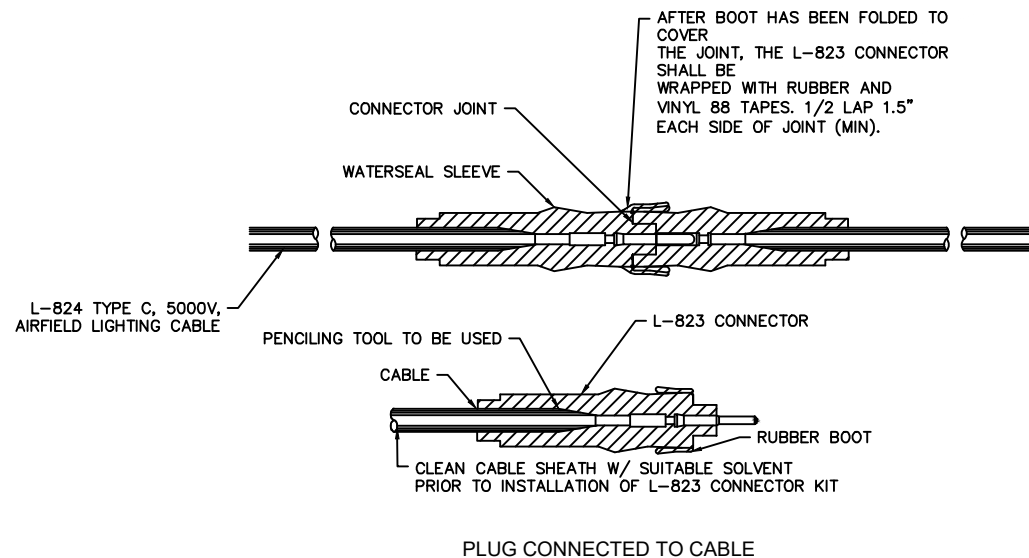
DATE  
24 FEBRUARY 2022

DRAWING NO.

SHEET NUMBER  
19 OF 21

**E102**

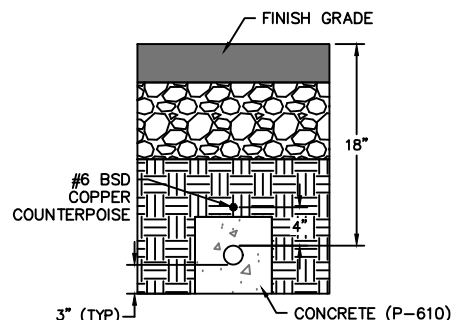




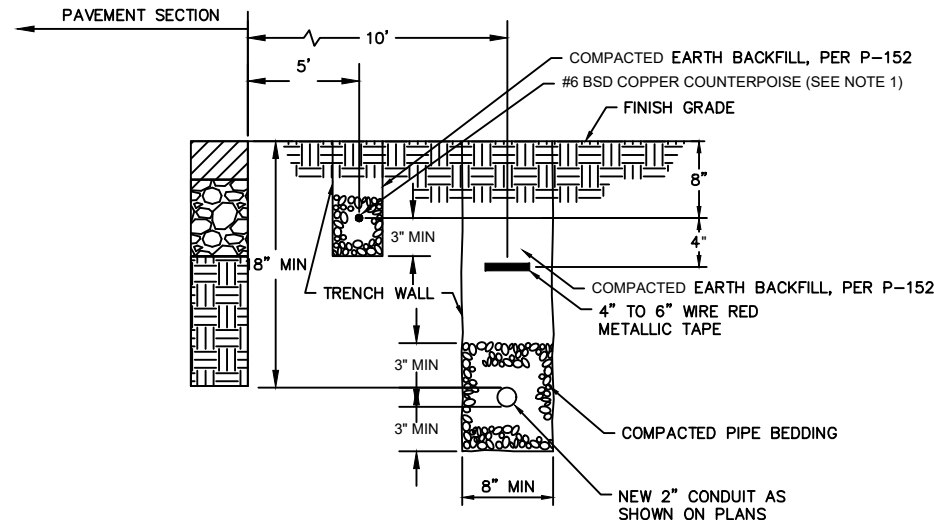
NOTES:

1. CONTRACTOR SHALL PROVIDE CABLE CIRCUIT IDENTIFICATION TAGS ATTACHED TO BOTH SIDES OF EACH CABLE CONNECTION.
2. INSTALLATION OF THE PIN/RECEPTACLE SHALL BE COMPLETED WITH A "CRIMPING" TOOL SUPPLIED OR RECOMMENDED BY THE MANUFACTURER AND DESIGNED FOR THIS SPECIFIC PURPOSE.

E1 L-823 CONNECTOR DETAIL  
NTS



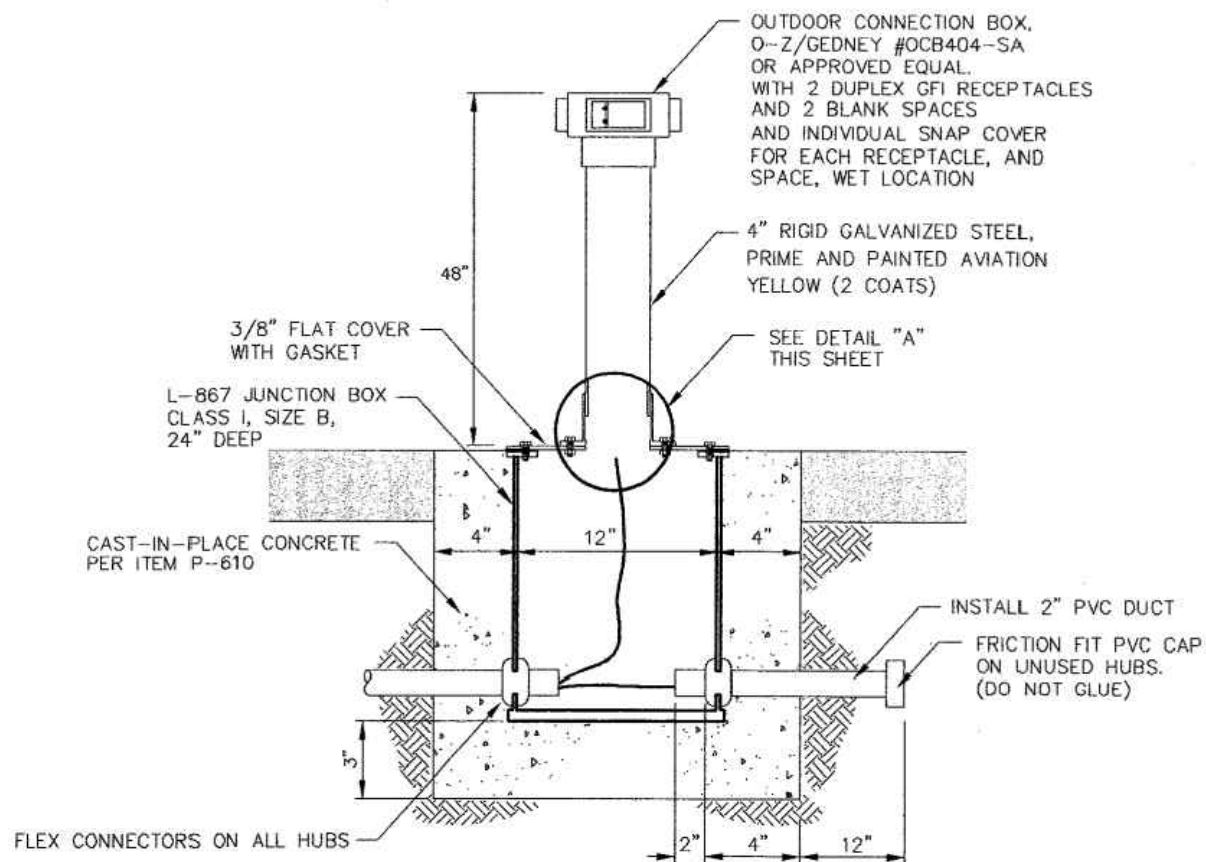
E2 1 WAY 2" CONDUIT CONCRETE ENCASED  
NTS



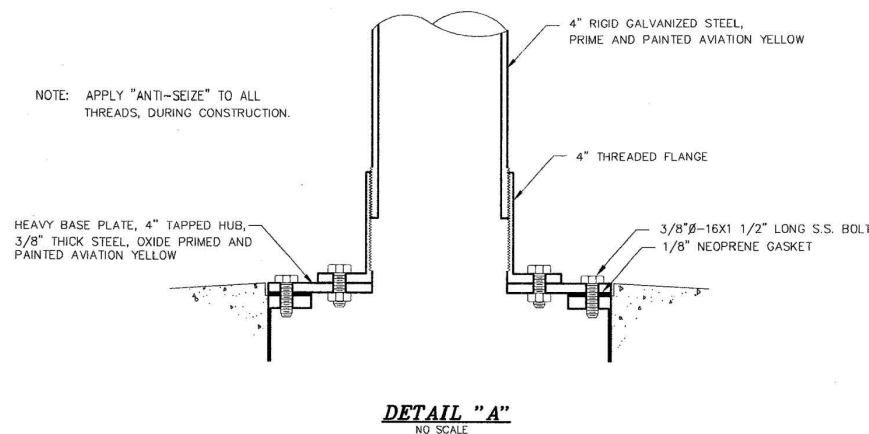
NOTES:

1. CONTRACTOR SHALL EXOTHERMICALLY WELD PROPOSED COUNTERPOISE TO EXISTING. IF EXISTING CANNOT BE FOUND, TERMINATE ENDS OF COUNTERPOISE WITH 3/4" x 10' COPPER CLAD GROUND ROD VIA EXOTHERMIC WELD.

E3 1 WAY 2" CONDUIT DIRECT EARTH BURIED - IN TURF  
NTS



E4 CONVENIENCE OUTLET BOLLARD INSTALLATION ELEVATION VIEW  
NTS



DETAIL "A"  
NO SCALE



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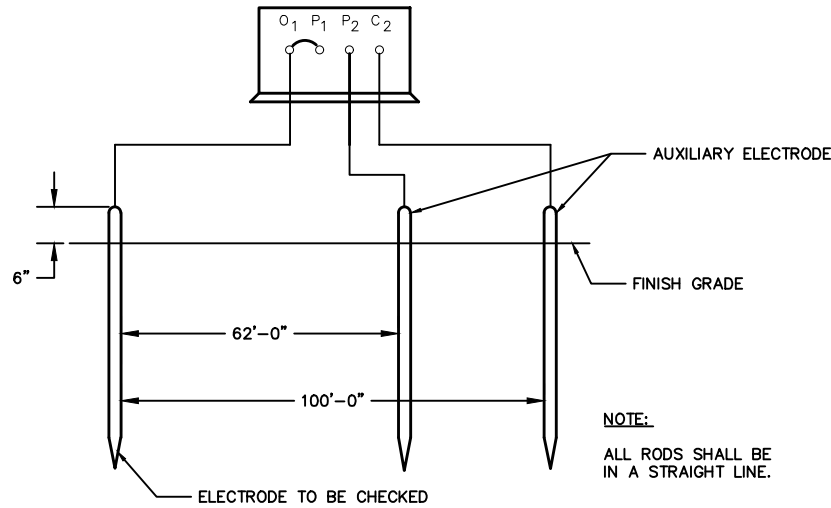
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**AUBURN  
MUNICIPAL AIRPORT**  
Auburn, California, 95603  
(530) 823-4211  
PLANNING AND PUBLIC WORKS

PROJECT TITLE:  
**HELICOPTER  
PARKING  
AREA**  
AUBURN MUNICIPAL AIRPORT  
AUBURN, CALIFORNIA

DRAWING TITLE:  
**LIGHTING  
DETAILS**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>E103</b>
SHEET NUMBER 20 OF 21	



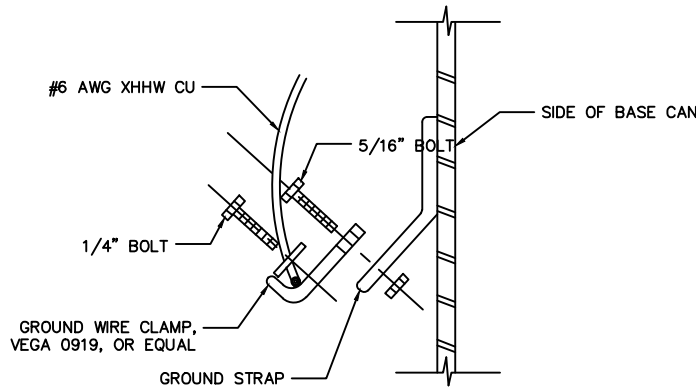


**GROUND ROD TEST:**

BEFORE ANY WIRE IS CONNECTED TO THE GROUND RODS, EACH ROD SHALL BE TESTED IN THE PRESENCE OF THE ENGINEER. A WRITTEN RECORD OF THE RESULTS OF EACH INDIVIDUAL TEST SHALL BE PREPARED AND SIGNED BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER FOR APPROVAL. A DIRECT READING, SINGLE TEST, PORTABLE GROUND TESTING METER SHALL BE USED TO TEST EACH GROUND ROD. THE AUXILIARY OR REFERENCE GROUND RODS SHALL BE 5/8" COPPER CLAD STEEL, NOT LESS THAN 4'-0" IN LENGTH, AND DRIVEN IN 3'-6" DEEP, AND SHALL BE INSTALLED IN A STRAIGHT LINE FROM THE GROUND ROD BEING TESTED. NO. 12 STRANDED WIRE LEADS WITH HIGH GRADE INSULATION SHALL BE CONNECTED TO THE ROD BEING TESTED, THE TWO REFERENCE RODS, AND TO THE PROPER GROUNDING POST ON THE INSTRUMENT. THE METER INDICATES THE RESISTANCE TO EARTH IN OHMS. THESE TESTS SHALL NOT BE MADE WITHIN 48 HOURS AFTER RAINFALL OR DURING RAINY OR FOGGY WEATHER. IN ADDITION, AFTER GROUND RODS ARE TESTED AND CONNECTED, A COMPLETE SYSTEM TEST SHALL BE MADE IN A SIMILAR MANNER, USING THE SAME METHOD, AND THE SYSTEM TEST SHALL NOT EXCEED 25 OHMS. GROUND ROD TESTING SHALL BE COMPLETED AND APPROVED PRIOR TO PLACEMENT OF CONCRETE PAVEMENT.

**E1 GROUND TEST - FALL OF POTENTIAL METHOD**

NTS

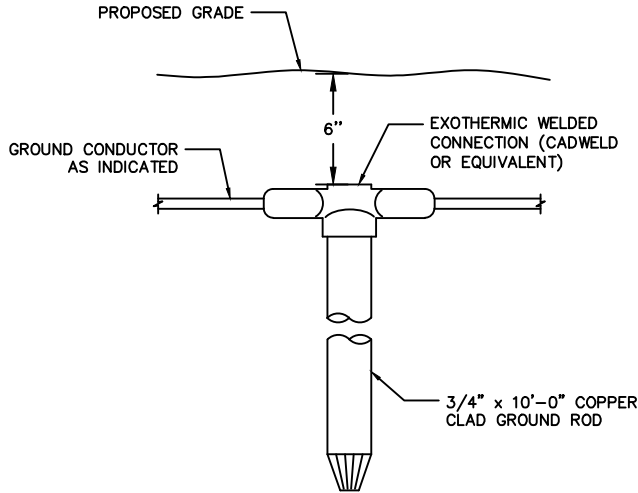


**NOTES:**

1. LOCATE GROUND STRAP ON INTERIOR WALL OF BASE CAN.

**E3 FACTORY INTERIOR GROUND LUG DETAIL**

NTS

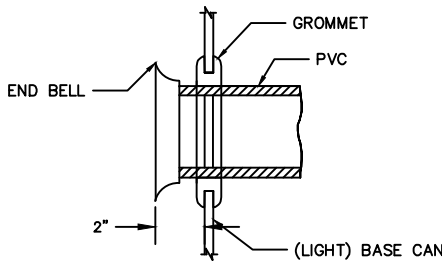


**NOTES:**

1. TOP OF GROUND ROD SHALL BE SAND BEDDED 6" MIN BELOW GRADE.
2. MAXIMUM 500' SPACING BETWEEN COUNTERPOISE GROUND RODS FOR NEW INSTALLATIONS.

**E2 GROUND ROD CONNECTION DETAIL**

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**E4 CONDUIT ENTRANCE TO BASE CAN DETAIL**

NTS



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**CITY OF AUBURN**

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AUBURN, CALIFORNIA

DRAWING TITLE:  
**LIGHTING  
DETAILS**

DATE 24 FEBRUARY 2022	DRAWING NO. <b>E104</b>
SHEET NUMBER 21 OF 21	